

Capital Improvements Committee

April 17, 2018

MEETING SUMMARY

A meeting of the Capital Improvement Committee of Oshtemo Township was held on Tuesday, April 17, 2018 at 1:00 pm in the main conference room of Oshtemo Township, 7275 West Main Street, Kalamazoo, Michigan. Persons in attendance included Libby Heiny-Cogswell, Marc Elliott, Jim Porter, Jamie Baker, Tom Wheat, Jim Hoekstra, Carl Benson, and Bernie Mein.

1. Welcome/Introductions/Call to Order: Libby Heiny-Cogswell called the meeting to order and welcomed members and guests.
2. Meeting Summaries: The meeting summary for the January 16, 2018 meeting was reviewed. The meeting minutes were approved by members upon motion by Mein and support by Baker.
3. Public Sewer Extensions, Phase 1 Project Update: Members were briefed on project progress in regard to financing, engineering-design, and easement acquisitions for the planned 2019 sewer extensions in the affected neighborhoods. The Township is still awaiting formal notification of funding from the USDA. Conceptual engineering design work has commenced for the six Phase I neighborhoods. The conceptual design work has enabled identification of the locations for the two sanitary sewer lift stations. The owners of the properties near the two lift stations will be approached to gauge interest in providing easements for the lift stations.
4. Potential Neighborhood Sidewalk Extensions: There is an opportunity to build sidewalks with the sewers in the affected neighborhoods. A preliminary concept plan was presented to the board on April 10th per the Board's inquiry to possibly fast-track select sidewalk projects. The Board requested meetings with the neighborhoods to solicit their input. The meetings with the residents are scheduled for early May.
5. Standard Specifications for Non-Motorized Facilities: The Township's specification for sidewalks is to extend the concrete walkway through driveways. The standard does not differentiate new construction from retrofit construction through existing drives. For retrofit construction, it was decided to extend the sidewalks through residential driveways; however, due to increased traffic at commercial driveways, it was decided not to extend the sidewalks through commercial driveways. Stopping the sidewalk at the commercial driveway will give a visual indication to the pedestrian to be more aware of vehicles crossing their walking path.

6. Drake Road Non-Motorized Project, W Main to Stadium Drive: Staff provided project updates on the three phases of the Drake Road non-motorized project. The northern portion, from West Main to Green Meadow, should be approved for funding and constructed this summer. The remaining two portions, Green Meadow to Stadium, are dependent on Amtrak's approval of the railroad crossing. The process to obtain Amtrak's approval could take years.

7. 2018 Roadway Projects within Oshtemo: Staff prepared an updated, informational map and listing of roadway maintenance projects in Oshtemo by the various lead agencies.

8. Mandatory Sewer Connection Notifications: Staff summarized the progress of the mandatory sewer connection notices. About 120 of 320 notices have been sent so far. Approximately 30 notices are sent per month to ensure staff have time to interact with each resident as they receive the notices.

9. Spring 2018 Hydrant Flushing Schedule: Staff presented a schedule and map of the City's 2018 hydrant flushing plan.

10. Go! Green Oshtemo: Members were briefed on a February "Go! Green Oshtemo" community input meeting which kicked-off a comprehensive planning project for parks. Non-motorized facilities and recreational trailway linkages are a major focus. Individual study groups for parks, non-motorized, cemeteries, and conservation will have kick-off meetings in April.

11. Miscellaneous Project/Program Updates & General Information. An overview of current activities and project status were presented, to include:

- KLA Group Public Water Extension & New Pressure Reduction Station
- BTR 2.0, Drake Road (WMU project with Oshtemo's participation to extend public utilities)
- Stadium Drive Non-Motorized Project, 8th St to 11th St. (potential for MDOT grants)
- MDOT US 131 bridge replacement over KL Ave (request for Oshtemo participation)
- LED Street Lights Upgrade
- Fire Department Project to Replace Hydrant Signs with Wire-Markers/Hydrant-Whips
- Stormwater Asset Management and Wastewater (SAW)
- KATS/RCKC CIP Data Initiative (a prototype initiative using GIS technology to capture, communicate & coordinate planned (5-years) capital improvements within the public ROW)

12. Member Comments and Information Items: No further comments were presented.

13. Next Meeting: The next regularly scheduled meeting of the CIC for Oshtemo is July 17, 2018 at 1:00 pm.



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269-216-5220 Fax 269-375-7180 www.oshtemo.org

Capital Improvements Committee

*Tuesday, April 17, 2018
1:00 P.M.*

Meeting Agenda

1. Welcome/Introductions/Call to Order. The chair shall open the meeting and welcome members.
2. Meeting Summary. Members shall review a summary of the CIC meeting held January 16, 2018 (attached).
3. Resident Inquiry/Commentary Request. As a support-committee, work sessions of the CIC are nominally not consider public meetings. Members shall consider a resident's request to be recognized to address the topic of sidewalk extensions into existing neighborhoods (item 4/5).
4. Public Sewer Extensions, Phase 1 Project Update. Members will be briefed regarding project progress in financing, engineering-design, and easement acquisitions for the planned 2019 sewer extensions in to developed neighborhoods (attached map).
5. Board consideration of potential neighborhood sidewalk extensions (in conjunction with Phase 1 sewer extensions). A preliminary concept plan was presented to the board on April 10th per the Board's inquiry to possibly fast-track select sidewalk projects. (See included memo with attached support material.)
6. Standard Specifications for Non-Motorized Facilities. The Township's specification for sidewalks is to extend the concrete walkway through driveways. The standard does not differentiate new construction from retrofit construction through existing drives. Guidance and member comments will be solicited concerning this topic, to include decision-based standards that may include consideration of roadway gutter pans, driveway use, underlying zoning, or possibly local character & land use form. (standard specifications attached)
7. Drake Road Non-Motorized Project, W Main to Stadium Drive. In October 2013 the Board directed staff to initiate project planning for a non-motorized facility along the west side of Drake Road. This goal has evolved into three (3) MDOT grant-funded construction projects. Staff will provide project updates. (See supplemental memo on the Amtrak crossing.)
 - Green Meadows Dr. to W. Michigan Ave - Safety Grant (\$320,00); Act 51 Sponsor = City of Kalamazoo
 - W Main to Green Meadows – Transportation Alternatives Grant (002 - \$417,459); Act 51 Sponsor = RCKC
 - W Michigan Ave to Stadium Drive – Transportation Alternatives Grant (001 - \$135,772); Act 51 Sponsor = RCKC
 - Stadium Drive to Parkview (partial) – Private development of the WMU BTR 2.0
8. 2018 Roadway Projects within Oshtemo. Staff has prepared an updated, informational map and listing of roadway maintenance projects in Oshtemo by the various lead agencies.

9. Mandatory Sewer Connection Notifications. Staff will summarize progress and resulting community engagement in response to the notification letters being distributed to properties required to connect to available public sanitary sewer service.
10. Spring 2018 Hydrant Flushing Schedule. A schedule and description of the City's 2018 hydrant flushing schedule is attached.
11. Go! Green Oshtemo. Members will be briefed on a February "Go! Green Oshtemo" community input meeting which kicked-off a comprehensive planning project for parks. Non-motorized facilities and recreational trailway linkages are a major focus. (public notice attached)
12. Miscellaneous Project/Program Updates & General Information. An overview of current activities and project status will be presented, to include:
 - KLA Group Public Water Extension & New Pressure Reduction Station
 - BTR 2.0, Drake Road (WMU project with Oshtemo's participation to extend public utilities)
 - Stadium Drive Non-Motorized Project, 8th St to 11th St. (potential for MDOT grants)
 - MDOT US 131 bridge replacement over KL Ave (request for Oshtemo participation)
 - LED Street Lights Upgrade
 - Fire Department Project to Replace Hydrant Signs with Wire-Markers/Hydrant-Whips
 - Stormwater Asset Management and Wastewater (SAW)
 - KATS/RCKC CIP Data Initiative (a prototype initiative using GIS technology to capture, communicate & coordinate planned (5-years) capital improvements within the public ROW)
13. Member Comments & Information Items. Attendees will be asked to share items of interest to CIC members.
14. Next Meeting. The next regularly scheduled meeting of the Oshtemo Township Capital Improvements Committee (CIC) will be held July 17, 2018 at 1:00 PM.

Capital Improvements Committee

January 16, 2018

MEETING SUMMARY

A meeting of the Capital Improvement Committee of Oshtemo Township was held on Tuesday, January 16, 2018 at 1:00 pm in the north conference room of Oshtemo Township, 7275 West Main Street, Kalamazoo, Michigan. Persons in attendance included Dave Bushouse (arriving late), Libby Heiny-Cogswell, Nancy Culp, Marc Elliott, Jim Porter, Jamie Baker, Tom Wheat, Mark Worden, Jim Hoekstra, Dick Hertzell, and Bernie Mein.

1. Welcome/Introductions/Call to Order: Libby Heiny-Cogswell called the meeting to order and welcomed members and guests.
2. Meeting Summaries: The meeting summary for the October 31, 2017 meeting was reviewed and two changes were recommended:
 - Adding Dick Hertzell to the list of attendees.
 - In section 6, second paragraph, changing the word “discussing” to “discussion.”

The meeting minutes were approved with the two changes by members upon motion by Baker and support by Mein.

3. 2018 Roadway Maintenance Projects: Marc Elliott informed the committee that the Township Board approved an increase in funds to fully match the PAR funds available from the RCKC. Marc Elliott and Mark Worden briefed the committee on the proposed 2018 Roadway Maintenance projects. After discussions between Township staff and RCKC staff, the list of 2018 projects for further consideration includes:
 - Toscana St (Tuscany Plat)
 - Forest Glen Estates
 - Oak Park Condominiums
 - St. Charles Drive and Belle Chase Blvd (Lasalle Homesites)
 - Rose Arbor No. 1 and No. 2
 - Maple Hill Drive south of M-43
 - Quail Run Drive
 - South 2nd Street between M-43 and West KL Ave
 - West J Ave between Wickford Ave and 2nd St (completing the 2017 project)
 - West H Ave between 3rd and 6th Streets

Additional field evaluation work is needed to determine the appropriate road repair method for each segment and the engineer's estimate of costs each segment. Staff will further refine the list of what can be accomplished within the 2018 PAR funds. This refined list will be distributed to CIC members to solicit further CIC member comment and consent in approximately two weeks.

For completeness, Jamie Baker informed the committee that primary county road projects, fully funded by the RCKC and planned for 2018 include:

- South 8th St between M-43 and West KL Ave. This road will be downgraded to a local road after it is reconstructed.
- West KL Ave between Drake Road and 11th Street. The road will be widened to three lanes with HMA overlay.
- West KL Ave between 11th Street and 9th Street. The road will be milled and HMA overlay.
- N Ave between 6th Street and 11th Street. Some portions will be chip sealed and fog sealed based on existing conditions. Other portions will receive mill and HMA overlay.
- Almena Drive, West KL Ave, and 1st Street Intersection Realignment. The two intersections will be realigned to 90 degree angles as a safety improvement. West KL Ave between 1st Street and Almena Drive will be abandoned.

4. Mandatory Sewer Connection Notification: Jamie Baker provided a summary of the mandatory sewer connection notification project. The first ten notifications were sent out on January 4, 2018. Baker explained what information was sent to the residents and what information is available on a Township webpage. The next set of notifications is planned for January 17, 2018.

5. Sidewalk Petition (Sewer Project Awareness): The engineering work to extend public sewers into older plats started this fall. As a courtesy to the residents, the Public Works Department sent a postcard notification to explain the presence of the survey crews in their neighborhoods. We also distributed information that the Township Board would welcome resident-initiated petitions for sidewalk extensions to be installed with the sewer project. Baker summarized the positive and negative reactions from residents and stated that no resident has volunteered to lead the petition drive for their neighborhood. Libby summarized the Township Board's intent to explore additional funding to build primary non-motorized facilities.

6. Contract to Install Water Service Connections (SWT): SWT excavating provided updated 2018 rates including:

- City of Kalamazoo mandated changed to Water Main Tap; 1-1/4 inch Corporation Stop requiring a \$43.89 increase
- RCKC ROW Permit fee increase from \$60 to \$95.

The committee discussed if the \$80 increase in SWT rates should result in a recommendation to the Township Board to increase the Water Curb Stop Installation fee increase. Based on the small number of curb stops installed each year by SWT, the committee does not recommend a change to the Water Curb Stop installation fee at this time. The change can be addressed during the next annual review of water connection fees.

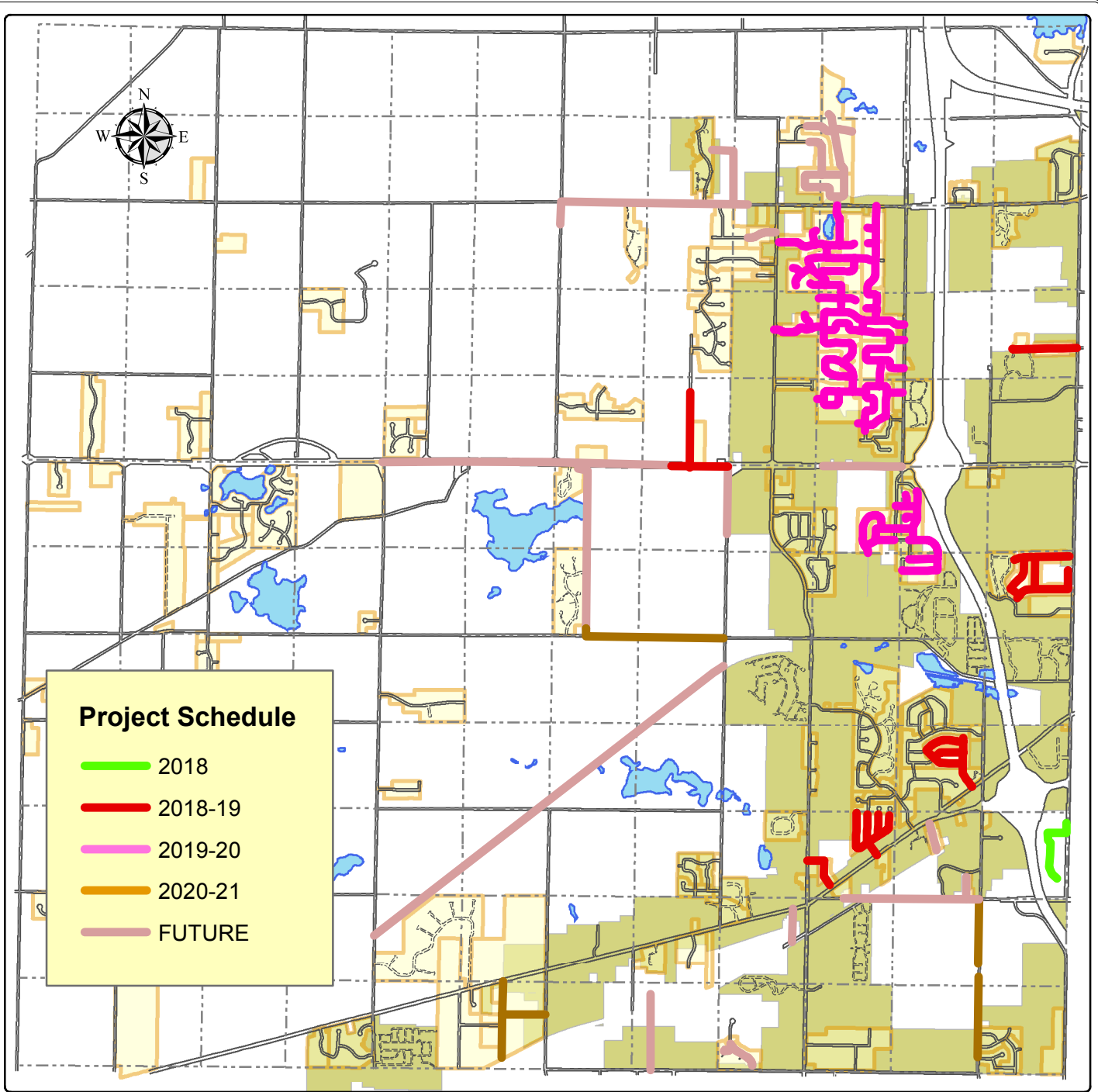
Mein motioned, seconded by Hertzell, to recommend to the Township Board to renew the SWT contract for 2018. The motion passed unanimously.

7. Miscellaneous Projects/Program Updates & General Information: An overview of current activities and project status was presented on the following topics:

- North 10th Street and KL Avenue & 8th Street Sewer projects
- KLA Group Water Extension and Pressure Reducing Station
- BTR 2.0, Drake Road
- Drake Road Non-Motorized Path
- RCKC abandonment of KL Avenue between 1st Street and Almena Drive
- MDOT US-131 bridge replacement over KL Avenue
- Fire Department hydrant inspections
- Storm and Wastewater (SAW) Grant
- Township meeting schedules
- Township committee memberships

8. Member Comments and Information Items: No further comments were presented.

9. Next Meeting: The next regularly scheduled meeting of the CIC for Oshtemo is April 17, 2018 at 1:00 pm.



Project Schedule

- 2018
- 2018-19
- 2019-20
- 2020-21
- FUTURE

Phased Extensions of Public Sanitary Sewer

Assumes USDA Financing (2019-2021)
through the Rural Development Program



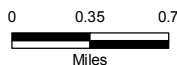
Preliminary Engineering Report
(Final Phasing As Submitted)

Charter Township of Oshtemo
Kalamazoo Co., Michigan

This map is for reference purposes only. While it is intended to be an accurate graphic representation, its accuracy cannot be guaranteed. Any conclusions or information derived from this map are at the users sole risk.

- Plats and Condos
- Sewer Service Available

Date: 2/2/2018



Memo



To: Oshtemo Charter Township Board
From: Marc Elliott, P.E., Director of Public Works
Date: April 5, 2018
Subject: Fast Track Sidewalks, Concept Plans & Probable Cost

OBJECTIVE

The Board shall review preliminary concept plans for six (6) sets of fast track residential sidewalk projects to be considered. These projects are potential sidewalk extensions within the six neighborhood sanitary sewer extensions to be built in 2019. The concept plans are intended to provide “core” connectivity within existing residential developments, with connection to the Township’s existing/planned through-corridor, non-motorized facilities.

Members shall also review the existing policy for financing roads, sidewalks and non-motorized projects.

BACKGROUND

The coordination of Capital Improvement Projects is a goal of the Township. In 2019 six neighborhoods are targeted to receive public sewer extensions. The sewer project includes replacing the roadway and extending sewer service leads to the ROW limits. The Board identified that there is opportunity to include select extensions of public sidewalks into the affected neighborhoods during this work. Economies of scale should produce significant cost savings. The coordinated and concurrent construction of both sewer and sidewalks will also minimize the disruption and inconvenience experienced by homeowners.

The attached estimates do not consider the economy of scale aspect. Additionally, the Township is planning meetings with each area to dialogue with residents about the schematic sidewalk layouts, policies, and funding. Following these meetings and associated feedback, the Board will be asked to consider more precise extents of any sidewalk extensions.

INFORMATION PROVIDED

- Oshtemo Complete Streets Policy
- Maple Hill Area (1 of 6)
- West Main Area (2 of 6)
- Skyridge Area (3 of 6)
- Fairgrove Area (4 of 6)
- Meridian Area (5 of 6)
- Whitegate Farms Area (6 of 6)
- Engineer’s Probable Cost of Construction (6 tables)
- Engineer’s Summary of Fees (excerpt—in form used by USDA to describe non-participating work)
- Road/Sidewalk/Shared-Use Path Assessment Policy

CHARTER TOWNSHIP OF OSHTEMO
KALAMAZOO COUNTY, MICHIGAN

COMPLETE STREETS POLICY RESOLUTION

December 9, 2014

WHEREAS, "Complete Streets" are defined as a design framework that enables safe and convenient access for all users, including pedestrians, bicyclists, transit riders, and drivers of all ages and abilities; and

WHEREAS, "Complete Streets" are achieved when transportation agencies routinely plan, design, construct, reconstruct, operate, and maintain the transportation network to improve travel conditions for bicyclists, pedestrians, transit, and freight in a manner consistent with, and supportive of, the surrounding community; and

WHEREAS, development of pedestrian, bicycle, and transit infrastructure offers long-term cost savings and opportunities to create safe and convenient non-motorized travel; and

WHEREAS, streets that support and invite multiple uses, including safe, active, and ample space for pedestrians, bicycles, and transit are more conducive to the public life and efficient movement of people than streets designed primarily to move automobiles; and

WHEREAS, increasing active transportation (e.g., walking, bicycling, and public transportation) offers the potential for improved public health, economic development, a cleaner environment, reduced transportation costs, enhanced community connections, social equity, and more livable communities; and

WHEREAS, Complete Streets allows people with disabilities, low-income residents, older adults, children, and other segments of the population who do not have consistent access to vehicular transportation the ability to travel freely throughout the community; and

WHEREAS, the State of Michigan has adopted complete streets legislation with the passing of Public Acts 134 and 135 of 2010 that require the Michigan Department of Transportation to consider all users in transportation related projects and work with locals, townships, cities, and villages to including planning for Complete Streets in their transportation programming; and

WHEREAS, PA 135 stipulates that before a city, village, or township approves any project that affects a roadway or transportation facility under the jurisdiction of another agency, it shall consult with that agency and agree on how to address the respective complete streets policies before approving a non-motorized project affecting a transportation facility whether it is under the jurisdiction of the Michigan Department of Transportation (MDOT), or under the jurisdiction of the county or another municipality; and

WHEREAS, PA 135 further stipulates that before MDOT submits its multiyear capital plan to the state transportation commission or a county road agency approves its multi-year capital plan, for any project that affects a roadway or transportation facility within or under the jurisdiction of a city, village, or township, the MDOT or county road agency shall consult with the affected city, village, or township and agree on how to address the respective complete streets; and

WHEREAS, the Oshtemo Charter Township's Master Plan was last updated in 2012 and addresses multiple forms of transportation including roads, mass transportation, and non-motorized transportation; and

WHEREAS, in 2012, the Township adopted a Non-Motorized Facilities Plan providing a vision and strategy for development and implementation of the community's goals for non-motorized facilities; and

WHEREAS, both the Kalamazoo Area Transportation Study (KATS) and MDOT have adopted Complete Streets Policies that incorporate consideration of these principles into broader transportation planning and funding decisions; and

WHEREAS, the Township seeks to provide a high quality of life for its residents consistent with the vision presented in the Master Plan as well an environment conducive to business development and will continue to communicate and coordinate with its road agencies and local and regional transportation partners to develop a more complete transportation system in concert with this and other similar policies.

NOW, THEREFORE, BE IT RESOLVED, that the Charter Township of Oshtemo Board of Trustees hereby declares its support of Complete Streets policies and to the extent feasible will incorporate Complete Streets design considerations and practices as a routine part of infrastructure planning and implementation; and

BE IT FURTHER RESOLVED, that the Charter Township of Oshtemo will consider Complete Street principles in future long-range planning documents, such as the Master Plan, Zoning Ordinance, Non-Motorized Plan, Recreation Plan, and on-going zoning activities such as site plan review; and

BE IT FURTHER RESOLVED, that the Charter Township of Oshtemo will work closely with its local, regional, and State transportation partners in the planning, designing, construction, operation, and maintenance of the transportation network in the Township to achieve compliance with this Policy.

A motion was made by Dusty Farmer, seconded by Lee Larson, to adopt the foregoing Resolution.

Upon a roll call vote, the following voted "Aye":

Elizabeth Heiny-Cogswell, Deborah Everett, Nancy Culp, Dusty Farmer, Lee Larson, Nancy Carr and Grant Taylor

The following voted "Nay": None

The following "Abstained": None

The Supervisor declared that the Resolution has been adopted.



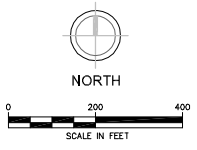
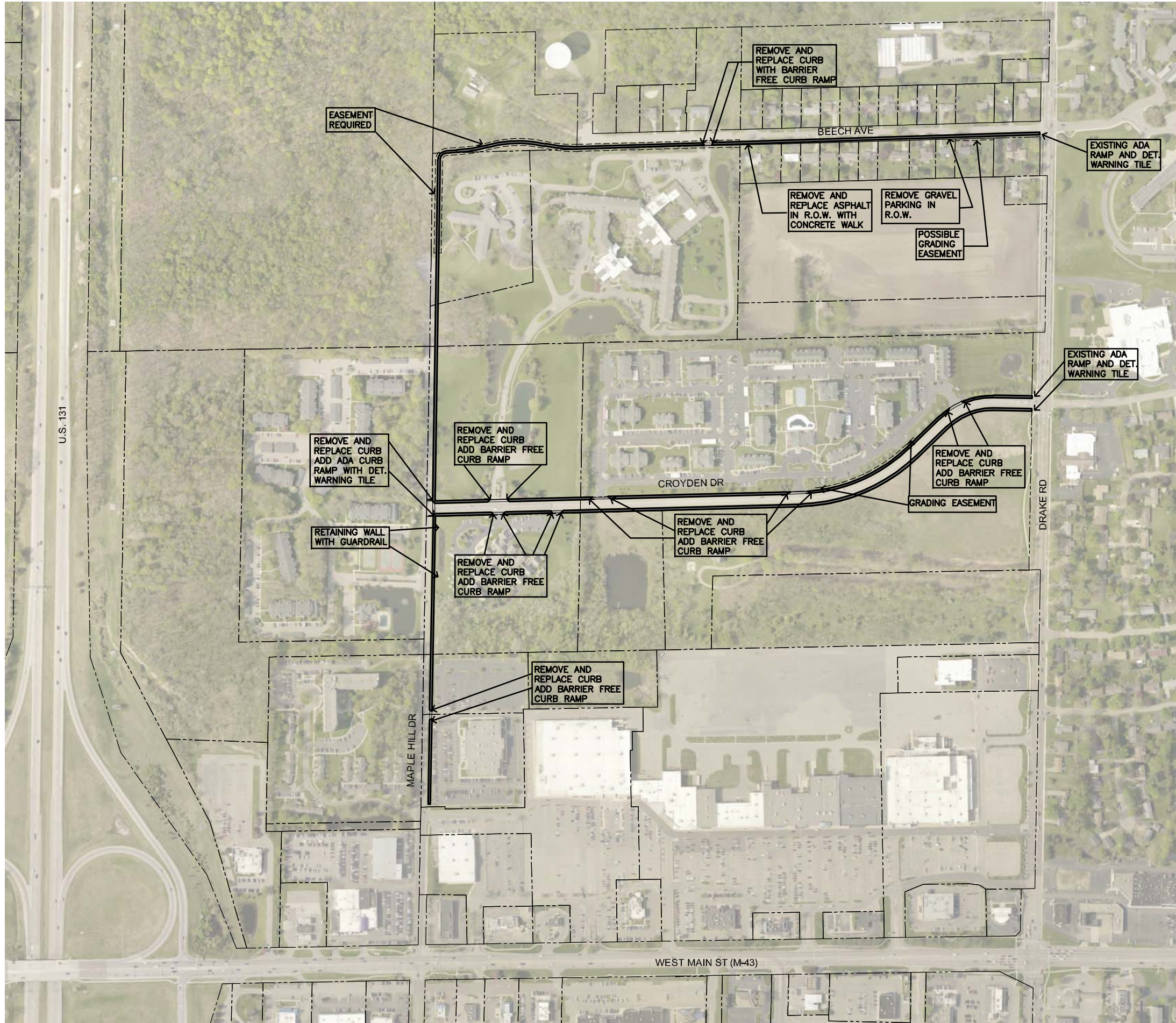
DEBORAH L. EVERETT, Clerk
Oshtemo Charter Township

CERTIFICATE

I hereby certify that the foregoing constitutes a true and complete copy of an Excerpt of the Minutes of a meeting of the Oshtemo Charter Township Board, held on December 9, 2014, at which meeting all members were present and voted upon the same as indicated in said Minutes; that said meeting was held in accordance with the Open Meetings Act of the State of Michigan.

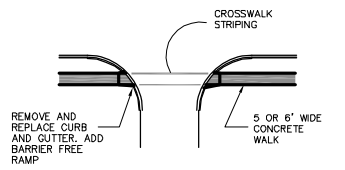


DEBORAH L. EVERETT, Township Clerk

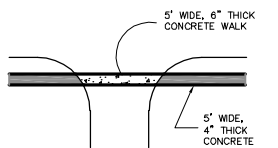


LEGEND

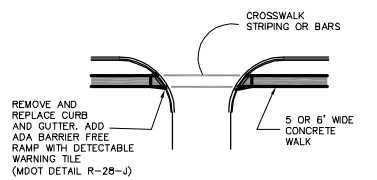
PROPOSED APPROXIMATE SIDEWALK LOCATION
5 OR 6' WIDE CONCRETE WALK



CROSSWALK AT COMMERCIAL DRIVE - TYPICAL
NOT TO SCALE



CROSSWALK AT RESIDENTIAL DRIVE - TYPICAL
NOT TO SCALE



CROSSWALK AT PUBLIC STREET R.O.W. - TYPICAL
NOT TO SCALE

6' WIDE CONCRETE WALK - CROYDEN DRIVE, MAPLE HILL DRIVE
5' WIDE CONCRETE WALK - BEECH AVENUE, WEST CONNECTOR



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Tel: (269) 382-8972

Landscape Architecture
Urban Planning
Park & Recreation
Comps & Institutional Planning
Camp Planning & Design

REVISIONS

OSHTEMO TOWNSHIP
OSHTEMO TOWNSHIP, MICHIGAN
NEIGHBORHOOD SIDEWALK EXTENSION - PHASE 1
MAPLE HILL AREA

DESIGN TEAM
MGL, RCC

CHECK BY

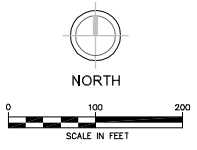
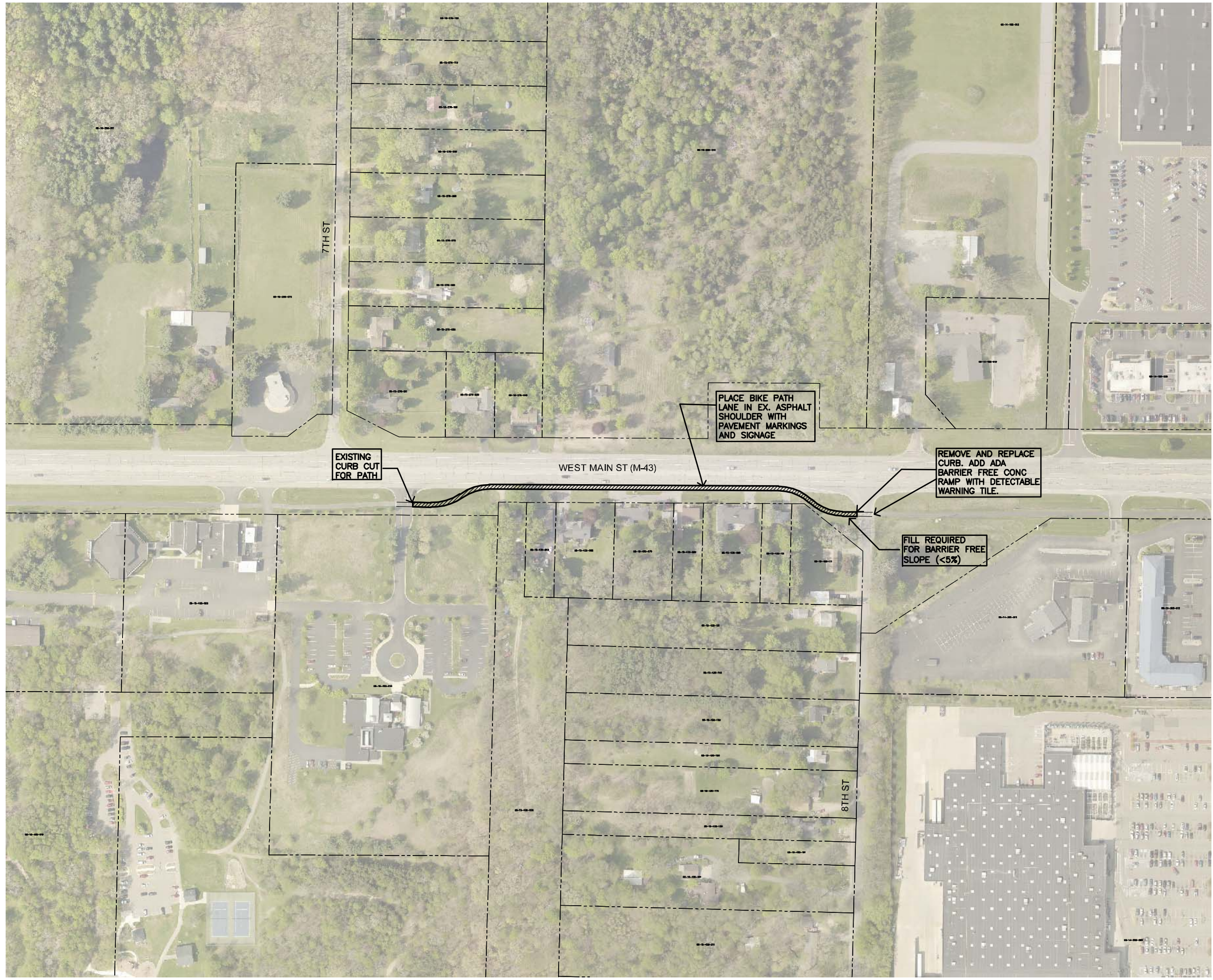
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NOT FOR CONSTRUCTION

APRIL 10, 2018

RAW PROJECT NO.
P13719

1/6



LEGEND
 PROPOSED APPROXIMATE SIDEWALK LOCATION
 10' WIDE ASPHALT

FLS WANDERBANK
 FEDERAL SAVINGS CORPORATION
 4738 Campus Drive
 Kalamazoo, MI 49008
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 F: 269.382.0972

ocba
 landscape architects
 605 South Riverdale Drive
 Kalamazoo, Michigan 49006
 Phone: (269) 381-9977
 Fax: (269) 381-2844

Landscape Architecture
 Urban Planning
 Parks & Recreation
 Campus & Institutional Planning
 City Planning & Design

OSHTEMO TOWNSHIP
 OSHTEMO TOWNSHIP, MICHIGAN
 NEIGHBORHOOD SIDEWALK EXTENSION - PHASE 1
 WEST MAIN AREA

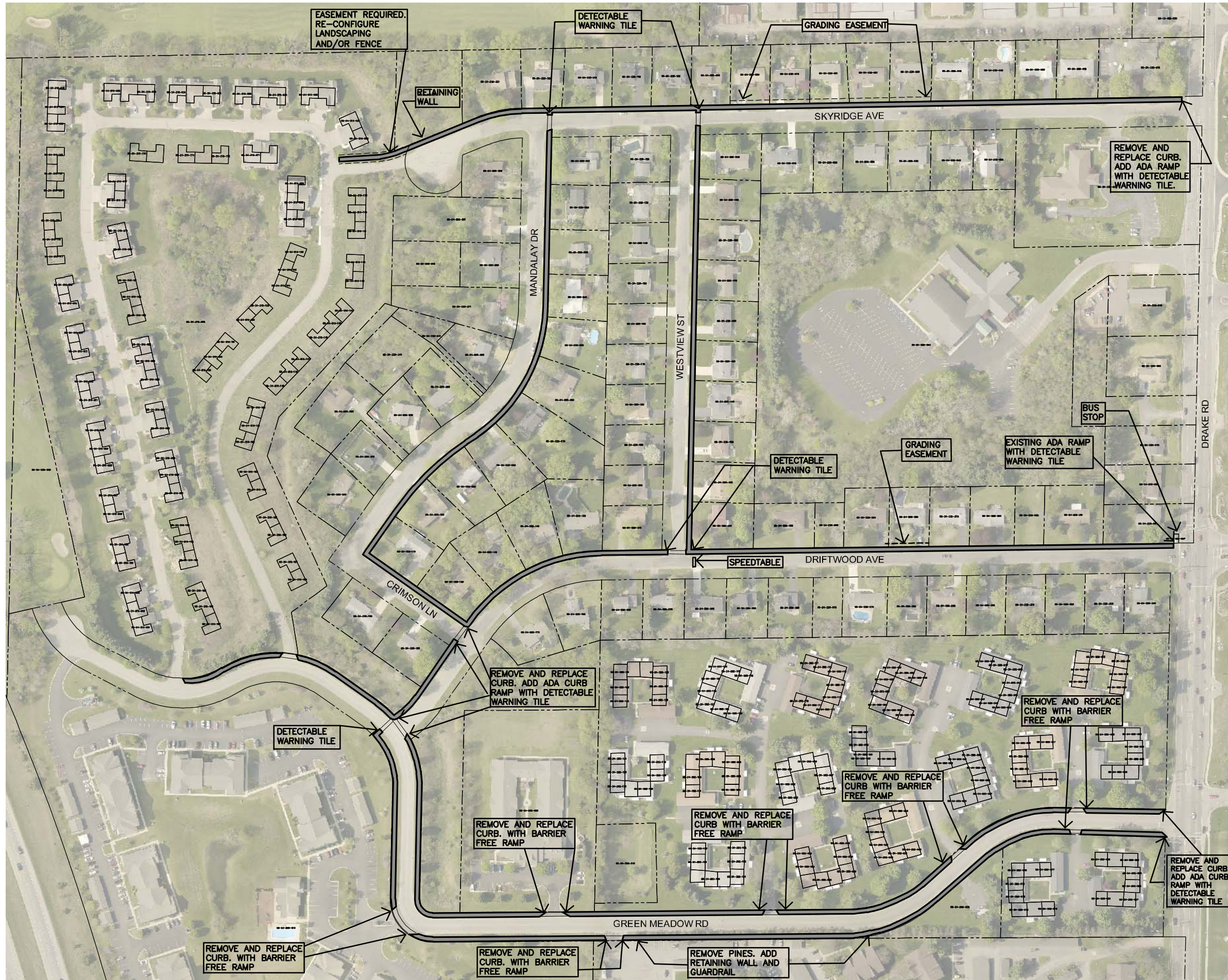
DESIGN TEAM
 MGL, RCC
 CHECK BY

DRAWING INFORMATION
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NOT FOR CONSTRUCTION

APRIL 10, 2018
 PAV PROJECT NO.
 P-13719

2/6



NORTH

0 100 200
SCALE IN FEET

LEGEND

- PROPOSED APPROXIMATE SIDEWALK LOCATION
5' OR 6' WIDE CONCRETE WALK
- CROSSWALK STRIPING
- REMOVE AND REPLACE CURB AND GUTTER. ADD BARRIER FREE RAMP
- 5' OR 6' WIDE CONCRETE WALK
- CROSSWALK AT COMMERCIAL DRIVE - TYPICAL
NOT TO SCALE
- 5' OR 6' WIDE, 6" THICK CONCRETE WALK
- 5' OR 6' WIDE, 4" THICK CONCRETE WALK
- CROSSWALK AT RESIDENTIAL DRIVE - TYPICAL
NOT TO SCALE
- CROSSWALK STRIPING OR BARS
- REMOVE AND REPLACE CURB AND GUTTER. ADD ADA BARRIER FREE RAMP WITH DETECTABLE WARNING TILE (DOT DETAIL R-28-J)
- 5' OR 6' WIDE CONCRETE WALK
- CROSSWALK AT PUBLIC STREET R.O.W. - TYPICAL
NOT TO SCALE
- 6' WIDE CONCRETE WALK
- 5' WIDE CONC. WALK
- GREEN MEADOW ROAD
- SKYRIDGE AVENUE
- MANDALAY DRIVE
- WESTVIEW STREET
- DRIFTWOOD AVENUE

FLS LANDBANK
FEDERAL LAND BANK OF MICHIGAN

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REVISION

**OSHTEMO TOWNSHIP
OSHTEMO TOWNSHIP, MICHIGAN
NEIGHBORHOOD SIDEWALK EXTENSION - PHASE 1
SKYRIDGE AREA**

DESIGN TEAM
MGL, RCC

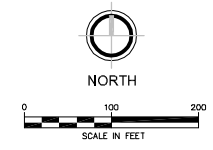
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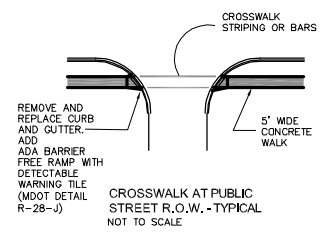
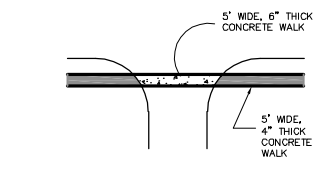
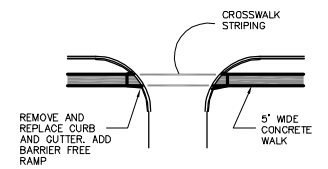
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P&W PROJECT NO.
P13719

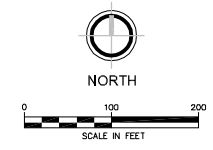
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LEGEND

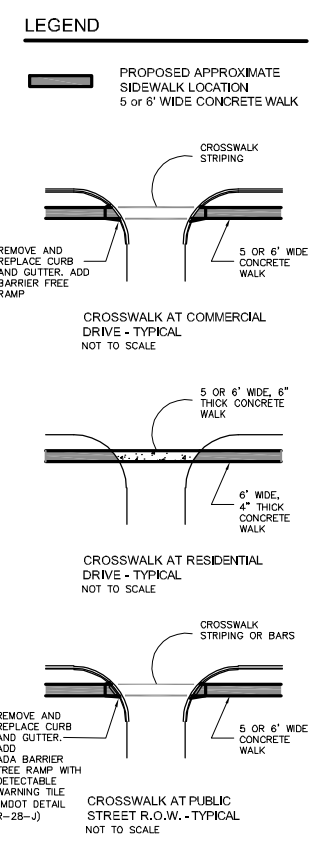
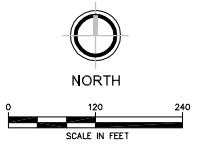
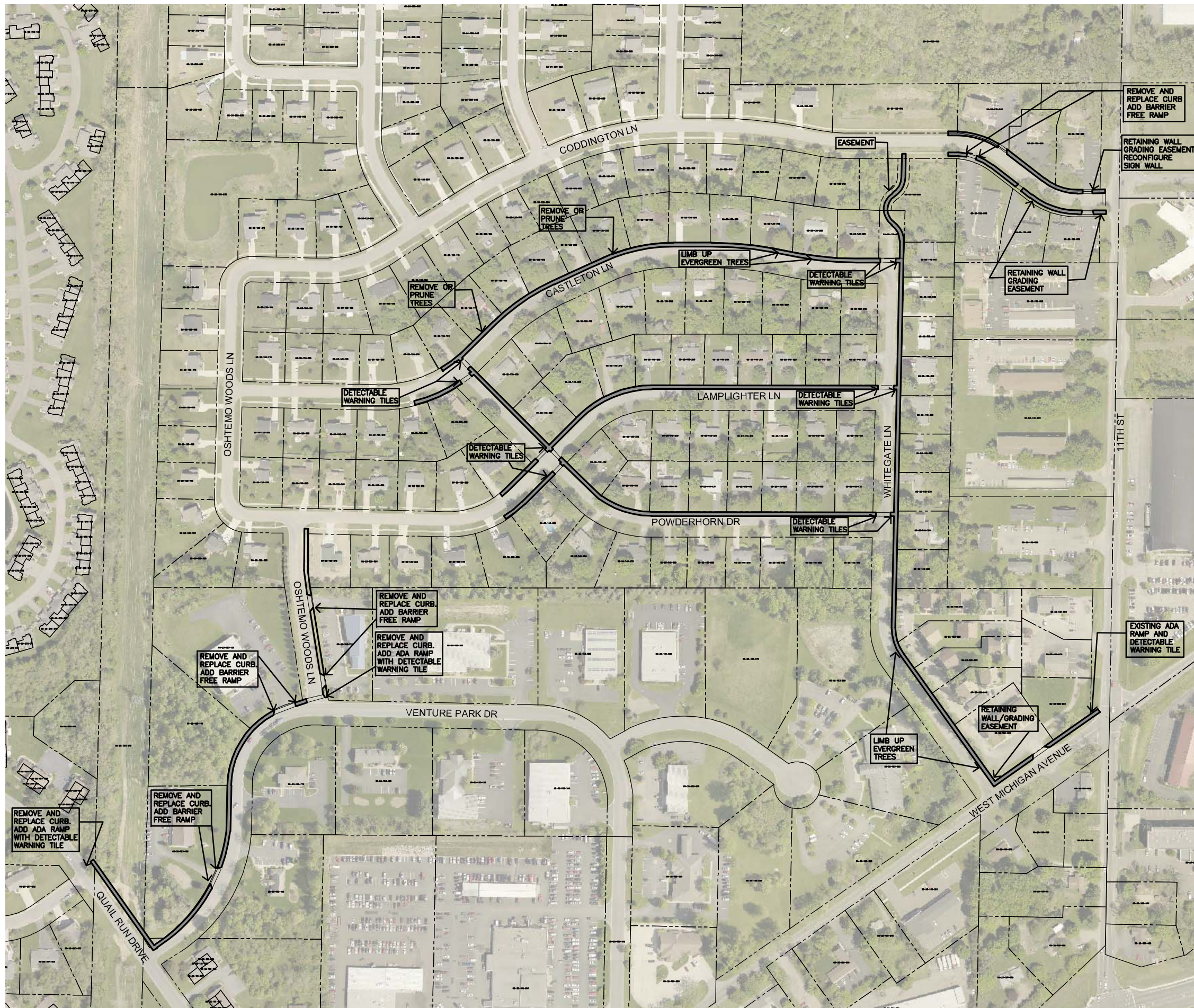
— PROPOSED APPROXIMATE SIDEWALK LOCATION
 5' WIDE CONCRETE WALK





LEGEND

- 
 PROPOSED APPROXIMATE SIDEWALK LOCATION
 5' WIDE CONCRETE WALK
- 
 CROSSWALK STRIPING
- 
 REMOVE AND REPLACE CURB AND GUTTER, ADD BARRIER FREE RAMP
 CROSSWALK AT COMMERCIAL DRIVE - TYPICAL
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 5' WIDE, 6" THICK CONCRETE WALK
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- 
 CROSSWALK STRIPING OR BARS
 REMOVE AND REPLACE CURB AND GUTTER, ADD ADA BARRIER FREE RAMP WITH DETECTABLE WARNING TILE (MOT DETAIL R-28-J)
 CROSSWALK AT PUBLIC STREET R.O.W. - TYPICAL
 NOT TO SCALE



- 6' WIDE CONCRETE WALK
- WEST MICHIGAN AVENUE
 - VENTURE PARK DRIVE
 - OSHTEMO WOODS LANE
- 5' WIDE CONCRETE WALK
- WHITEGATE LANE
 - QUAIL RUN DRIVE
 - POWDERHORN DRIVE
 - LAMPLIGHTER LANE
 - CASTLETON LANE
 - CODDINGTON LANE

FLS LANDBANK
RESIDENTIAL SERVICES OPERATIONS

4798 Campus Drive
Kalamazoo, MI 49008
P: 269.385.0011
F: 269.382.6972

OSHTEMO TOWNSHIP
OSHTEMO TOWNSHIP, MICHIGAN
NEIGHBORHOOD SIDEWALK EXTENSION - PHASE 1
WHITEGATE FARMS AREA

DESIGN TEAM
M&J, RCC

CHECK BY

DRAWING INFORMATION
833250, SW 5
040518 pww/mjk

NOT FOR CONSTRUCTION

APRIL 10, 2018
REV PROJECT NO.
P13719

6/6

10 April 2018



Neighborhood Sidewalk Concept Plan
Oshtemo Township, Michigan

Preliminary Opinion of Probable Costs

Item	Qty.	Unit	Unit Price	Total
Sheet 1 : Maple Hill Area				
Tree and Brush Removal	1	LS	\$ 5,000.00	\$ 5,000.00
Sawcut and Remove Paving (Residential Driveways)	220	LF	\$ 10.00	\$ 2,200.00
4" Concrete Walk	55,300	SF	\$ 8.00	\$ 442,400.00
6" Concrete Walk	1,100	SF	\$ 10.00	\$ 11,000.00
Remove and Replace Curb and Gutter	256	LF	\$ 20.00	\$ 5,120.00
Detectable Warning Tiles	24	SF	\$ 50.00	\$ 1,200.00
Segmental Unit Retaining Wall	700	SF	\$ 40.00	\$ 28,000.00
Pedestrian Crosswalk Striping	900	LF	\$ 3.00	\$ 2,700.00
Vinyl Coated Chain Link Fence 4' ht Guardrail	230	LF	\$ 22.00	\$ 5,060.00
Traffic Control	1	LS	\$ 5,000.00	\$ 5,000.00
Soil Erosion Control	1	LS	\$ 2,500.00	\$ 2,500.00
Topsoil and Turf Restoration	19,400	LF	\$ 6.00	\$ 116,400.00
Sheet 1 Subtotal				\$ 626,580.00
Engineering (20%)				\$ 125,316.00
Administration and Legal (5%)				\$ 31,329.00
Contingency (10%)				\$ 62,658.00
Sheet 1 Total				\$ 845,883.00
Sheet 2 : West Main Area				
Remove and Replace Curb and Gutter	32	LF	\$ 20.00	\$ 640.00
MDOT Class II Fill Compacted	40	CY	\$ 18.00	\$ 720.00
Detectable Warning Tiles	40	SF	\$ 50.00	\$ 2,000.00
Asphalt Path (10' wide)	320	SY	\$ 18.00	\$ 5,760.00
Crosswalk and Bike Lane Striping / Markings	1,500	LF	\$ 3.00	\$ 4,500.00
Bike Lane Signs	2	EA	\$ 600.00	\$ 1,200.00
Traffic Control	1	LS	\$ 1,000.00	\$ 1,000.00
Soil Erosion Control	1	LS	\$ 500.00	\$ 500.00
Topsoil and Turf Restoration	600	LF	\$ 6.00	\$ 3,600.00
Sheet 2 Subtotal				\$ 19,920.00
Engineering (20%)				\$ 3,984.00
Administration and Legal (5%)				\$ 996.00
Contingency (10%)				\$ 1,992.00
Sheet 2 Total				\$ 26,892.00

10 April 2018



Neighborhood Sidewalk Concept Plan
Oshtemo Township, Michigan

Preliminary Opinion of Probable Costs

Item	Qty.	Unit	Unit Price	Total
Sheet 3 : Skyridge Area				
Tree Removal	1	LS	\$ 10,000.00	\$ 10,000.00
Sawcut and Remove Paving (Residential Driveways)	780	LF	\$ 10.00	\$ 7,800.00
4" Concrete Walk	50,600	SF	\$ 8.00	\$ 404,800.00
6" Concrete Walk	3,900	SF	\$ 10.00	\$ 39,000.00
Remove and Replace Curb and Gutter	270	LF	\$ 20.00	\$ 5,400.00
Detectable Warning Tiles	170	SF	\$ 50.00	\$ 8,500.00
Segmental Unit Retaining Wall	1,500	SF	\$ 40.00	\$ 60,000.00
Vinyl Coated Chain Link Fence 4' ht Guardrail	700	LF	\$ 22.00	\$ 15,400.00
Pedestrian Crosswalk Striping	1,050	LF	\$ 3.00	\$ 3,150.00
Traffic Control	1	LS	\$ 10,000.00	\$ 10,000.00
Soil Erosion Control	1	LS	\$ 3,000.00	\$ 3,000.00
Topsoil and Turf Restoration	21,000	LF	\$ 6.00	\$ 126,000.00
Sheet 3 Subtotal				\$ 693,050.00
Engineering (20%)				\$ 138,610.00
Administration and Legal (5%)				\$ 34,652.50
Contingency (10%)				\$ 69,305.00
Sheet 3 Total				\$ 935,617.50
Sheet 4 : Fairgrove Area				
Landscape Removals	1	LS	\$ 500.00	\$ 500.00
Sawcut and Remove Paving (Residential Driveways)	450	LF	\$ 10.00	\$ 4,500.00
Remove and Replace Curb and Gutter	16	LF	\$ 20.00	\$ 320.00
4" Concrete Walk	12,400	SF	\$ 8.00	\$ 99,200.00
6" Concrete Walk	2,700	SF	\$ 10.00	\$ 27,000.00
Detectable Warning Tiles	40	SF	\$ 50.00	\$ 2,000.00
Pedestrian Crosswalk Striping	200	LF	\$ 3.00	\$ 600.00
Traffic Control	1	LS	\$ 3,000.00	\$ 3,000.00
Soil Erosion Control	1	LS	\$ 500.00	\$ 500.00
Topsoil and Turf Restoration	4,200	LF	\$ 6.00	\$ 25,200.00
Sheet 4 Subtotal				\$ 162,820.00
Engineering (20%)				\$ 32,564.00
Administration and Legal (5%)				\$ 8,141.00
Contingency (10%)				\$ 16,282.00
Sheet 4 Total				\$ 219,807.00

10 April 2018



Neighborhood Sidewalk Concept Plan
Oshtemo Township, Michigan

Preliminary Opinion of Probable Costs

Item	Qty.	Unit	Unit Price	Total
Sheet 5 : Meridian Area				
Landscape Removals	1	LS	\$ 500.00	\$ 500.00
Sawcut and Remove Paving (Residential Driveways)	150	LF	\$ 10.00	\$ 1,500.00
Remove and Replace Curb and Gutter	16	LF	\$ 20.00	\$ 320.00
4" Concrete Walk	5,800	SF	\$ 8.00	\$ 46,400.00
6" Concrete Walk	750	SF	\$ 10.00	\$ 7,500.00
Detectable Warning Tiles	10	SF	\$ 50.00	\$ 500.00
Pedestrian Crosswalk Striping	60	LF	\$ 3.00	\$ 180.00
Traffic Control	1	LS	\$ 1,500.00	\$ 1,500.00
Soil Erosion Control	1	LS	\$ 500.00	\$ 500.00
Topsoil and Turf Restoration	2,400	LF	\$ 6.00	\$ 14,400.00
Sheet 5 Subtotal				\$ 73,300.00
Engineering (20%)				\$ 14,660.00
Administration and Legal (5%)				\$ 3,665.00
Contingency (10%)				\$ 7,330.00
Sheet 5 Total				\$ 98,955.00
Sheet 6 : Whitegate Farms Area				
Landscape Removal and Pruning	1	LS	\$ 1,500.00	\$ 1,500.00
Sawcut and Remove Paving (Residential Driveways)	750	LF	\$ 10.00	\$ 7,500.00
4" Concrete Walk	36,800	SF	\$ 8.00	\$ 294,400.00
6" Concrete Walk	3,750	SF	\$ 10.00	\$ 37,500.00
Remove and Replace Curb and Gutter	170	LF	\$ 20.00	\$ 3,400.00
Detectable Warning Tiles	228	SF	\$ 50.00	\$ 11,400.00
Pedestrian Crosswalk Striping	964	LF	\$ 3.00	\$ 2,892.00
Traffic Control	1	LS	\$ 10,000.00	\$ 10,000.00
Soil Erosion Control	1	LS	\$ 3,000.00	\$ 3,000.00
Topsoil and Turf Restoration	14,000	LF	\$ 6.00	\$ 84,000.00
Sheet 6 Subtotal				\$ 455,592.00
Engineering (20%)				\$ 91,118.40
Administration and Legal (5%)				\$ 22,779.60
Contingency (10%)				\$ 45,559.20
Sheet 6 Total				\$ 615,049.20
PROJECT SUMMARY				
Sheet 1 : Maple Hill Area				\$ 845,883.00
Sheet 2 : West Main Area				\$ 26,892.00
Sheet 3 : Skyridge Area				\$ 935,617.50
Sheet 4 : Fairgrove Area				\$ 219,807.00
Sheet 5 : Meridian Area				\$ 98,955.00
Sheet 6 : Whitegate Farms Area				\$ 615,049.20
PROJECT GRAND TOTAL				\$ 2,742,203.70

RUS CERTIFICATION PAGEPROJECT NAME: Neighborhood Sewer Extensions Phase #1

The Engineer and Owner hereby concur in the Funding Agency required revisions to E-500. In addition, Engineer certifies to the following:

All modifications required by RUS Bulletin 1780-26 have been made in accordance with the terms of the license agreement, which states in part that the Engineer “must plainly show all changes to the Standard EJCDC Text, using ‘Track Changes’ (redline/strikeout), highlighting, or other means of clearly indicating additions and deletions.” Such other means may include attachments indicating changes (e.g. Supplementary Conditions modifying the General Conditions).

SUMMARY OF ENGINEERING FEES

Note that the fees indicated on this table are only a summary and if there is a conflict with any provision of Exhibit C, the provisions there overrule the values on this table. Fees shown in will not be exceeded without the concurrence of the Agency.

Basic Services	\$ <u>531,000</u>
Resident Project Observation	\$ <u>266,000</u>
Additional Services	\$ <u>124,000</u>
TOTAL:	\$ <u>921,000</u>

**CHARTER TOWNSHIP OF OSHTEMO
KALAMAZOO COUNTY, MICHIGAN**

**MOTION TO APPROVE
ROAD/SIDEWALK/SHARED-USE PATH
ASSESSMENT POLICY**

December 12, 2017

WHEREAS, the Charter Township of Oshtemo has heretofore adopted road assessments for local roads, platted streets, reconstruction or overlay of gravel, dust control on gravel roads, together with an assessing policy and administrative procedure; and

WHEREAS, the Charter Township of Oshtemo has adopted a Non-Motorized Facilities/Sidewalk Ordinance, being Ordinance No. 468, and Section IX allows the Township Board to establish the costs for construction, reconstruction or repairs.

WHEREAS, upon the recommendation of the Road Committee, it is the desire of the Township Board to amend the road assessments for local roads and platted streets as well as sidewalks and shared-use paths.

NOW, THEREFORE, until further modification the Township Board, road/sidewalk/shared-use path assessment charges within the Charter Township of Oshtemo shall be as follows:

Motion made by Nancy Culp, seconded by Zak Ford to adopt the foregoing Policy.

The following voted "Aye":


Elizabeth Heiny-Cogswell, Dusty Farmer, Nancy Culp, Deborah Everett,
Dave Bushouse, Zak Ford and Ken Hudok

The following voted "Nay": None

The following abstained: None

The following members were absent: None

The Supervisor declared the Motion carried and the Policy duly adopted.



Dusty Farmer, Clerk
Oshtemo Charter Township

CERTIFICATE

I, Dusty Farmer, the duly elected and acting Clerk of the Charter Township of Oshtemo, hereby certify that the foregoing motion to approve Road/Sidewalk/Shared-Use Path Assessments Policy was adopted by the Township Board of said Charter Township at a regular meeting of said Board held on December 12, 2017, at which meeting a quorum was present as hereinbefore set forth; that said Policy was ordered to take effect January 1, 2018.



Dusty Farmer, Township Clerk

**CHARTER TOWNSHIP OF OSHTEMO
KALAMAZOO COUNTY, MICHIGAN**

**ROAD – SIDEWALK -- SHARED USE PATH
ASSESSMENT POLICY**

EFFECTIVE: JANUARY 1, 2018

ADMINISTRATIVE PROCEDURE

With assistance of the Capital Improvements Committee, the Township Board shall annually determine which roads will be restored/reconstructed or which will receive Hot Mix Asphalt (HMA) or gravel overlay. Selection of projects and treatments shall be subject to available Township funds, budgeting and scheduling.

A project funding formula is herein outlined. Specific projects shall be approved by the Township Board. On projects with no petitions, a Township Board resolution is needed.

Resolutions or petitions for Assessment Districts shall be forwarded to the Road Commission for Public Hearing.

Township road dollars shall, in general, not be spent on roadway reconstruction if the local street is not served with sanitary sewer and sanitary sewer is available to be extended to service properties along the street. This deferment of other than routine road maintenance is to facilitate the coordination of roadway work with sanitary sewer extensions to utilize Township funds in the most efficient and effective manner. If work is deferred for a local street under this policy, said local street will be planned for sanitary sewer extension within three years.

FUNDING FORMULA

SUBJECT TO AVAILABLE FUNDS

1. Local Roads –

Chip Seal or Seal Coat

Township Funds as supplemented with Road Commission Par Funds

HMA Overlay/Reconstruction

Township Funds as supplemented with Road Commission Par Funds

2. Plat Streets –

Chip Seal or Seal Coat

Township Funds as supplemented with Road Commission Par Funds

Ultra-Thin HMA Paving

100% Of Additional Cost Over Township-Proposed Project shall be by special assessment. Cul-de-Sacs shall be assessed in a manner that approximates an equitable contribution for the "pie" shaped square yardage frontage each assessable parcel (i.e. approximately comparable to the ½ roadway width square yardage assessed on a two-sided street).

HMA Overlay/Reconstruction

Township Funds as supplemented with Road Commission Par Funds

3. Dust Control (gravel roads) –

100% Township Funds

4. Failed Condition Local Road –

The Failed Condition Local Road Policy of the Road Commission shall be utilized.

5. Sidewalks/Shared Use Paths –

\$30.00 Per Front Foot For Single-Family Residential Zoning

\$35.00 Per Front Foot For Commercial/Industrial Zoning

Note 1: If developed, actual land use shall take president when determining the sidewalk/path assessment. When the underlying zoning allows other than single-family residential, or when substantial frontage remains undeveloped, the Township Board may elect to enter into an agreement with the property owner to defer collection of the full assessment until the parcel further develops.

Note 2: An exception shall apply to single-family residential properties which are (a) located along North 10th Street, from W. Main to the Kal-Haven Trail, or (b) located along an "arterial roadway" as classified by the National Functional Classification (NFC) will not be assessed. This location-based exemption is intended to recognize the subordinate neighborhood-usage of the sidewalk/path, versus the facility's function as an arterial/connector.

6. Land Owner Initiated or Shared-Use Private Roads –

At its sole discretion, the Oshtemo Township Board may elect to facilitate creation of a special assessment district for private roadway improvements when shared-use or other unique conditions warrant the proposed improvements as a public interest. Properties in Private Roadway SAD's shall bare 100% of the cost for the improvements, and shall include reimbursement of the administrative costs incurred by the Township.

A petitioned Public Roadway SAD project is exempt from Township cost participation as outlined in this policy. Specifically, the Township Board shall separately determine to what extent, if any, the Township shall contribute to the project.

ASSESSING POLICY

1. The Township shall annually determine which roads will be restored/reconstructed or which will receive an HMA or gravel overlay subject to budget limitations.
2. Parcels or outlots not in a plat, but with frontage on a plat street would be assessed using the funding formula but not to exceed the largest assessment in the district.
3. Parcels located on a "curved corner" will be assessed using the funding formula but not to exceed the largest assessment in the district.
4. Assessments for condominium units will assessed using the funding formula and the amount will be divided equally among the number of units.
5. The Township has the right and responsibility to set an assessment district, when it has been determined a road must be overlaid or reconstructed and funding is needed.
6. If sidewalk/shared use path project costs are less, individual assessments will be credited accordingly.
7. If any development is required by zoning to provide a sidewalk/shared use path, the development bears the full cost.

8. The Township reserves the right, should the need arise, to revise this policy at any time and may establish assessment districts calling for abutting land owners to share in the construction, restoration/reconstruction, HMA or gravel overlay of any road, sidewalk or non-motorized facility. In addition, the Township may consider a petition for road improvements and the establishment of a special assessment as provided by state law.



OSHTEMO TOWNSHIP SIDEWALK/SHARED-USE PATH CONSTRUCTION STANDARDS

Adopted August 9, 2016

CONCRETE SIDEWALK CONSTRUCTION

The construction of Sidewalks and Shared-Use Paths within Oshtemo is managed through the Township's issuance of a Sidewalk/Non-Motorized Path Permit. The permitting process includes both a pre-pour inspection of the base and concrete forms, and a final project inspection for acceptance of the work. Concrete sidewalk shall conform to MDOT 2012 (or current edition) Standard Specifications for Construction Section 803, "Concrete Sidewalks, Sidewalk Ramps and Steps" and shall be a minimum of five (5) feet wide unless a different width is required by other Township ordinances or regulations.

Grade

The sidewalk shall be constructed to match the existing grade, or as noted on the construction drawings. The sidewalk will have a transverse slope either toward or away from the road to maintain existing drainage patterns. Minor fills and cuts will be made in the field during construction to provide smooth transition of the sidewalk and maintain existing drainage patterns.

Sub-base Preparation

Existing vegetation shall be removed and topsoil excavated to provide a four (4") inch sand sub-base for the proposed sidewalk. The existing sand sub-base shall be compacted to ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures. Where fill sand is required it shall be compacted to achieve ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures.

Concrete

Concrete shall meet the requirements for Grade P1 Concrete or Grade S2 Concrete as specified in the MDOT Standard Specifications for Construction Section 601, "Portland Cement Concrete Pavements". Concrete shall be six sack limestone mix and shall be air-entrained and shall have a compressive strength of not less than 3,500 pounds per square inch, within twenty-eight (28) days of paving. Other materials shall meet the requirements of the applicable portions of the MDOT Standard Specifications for Construction. All concrete sidewalks shall be paved with a single course of concrete. Sidewalks through driveways shall be six (6") inches thick. All other walks shall be four (4") inches thick.

Joints

Full depth transverse expansion joints shall be constructed perpendicular to the surface of the sidewalk at intervals not to exceed fifty (50') feet. Expansion joint material shall be one-half (1/2") inch pre-

molded expansion joints and shall be set 1/4" below the surface of the sidewalk. Sealing of joints will not be required. One (1") inch pre-molded expansion joints must be placed between the sidewalk and back-of-curb when sidewalk is constructed between the curb and building or other rigid structures. Sealing of joints will not be required. Transverse plane of weakness joints shall be true to line and grade, and shall be placed at four (4') foot intervals and shall be formed with a grooving tool. Planes of weakness joints shall be constructed to a depth of at least one (1") inch and a width of 1/8 inch to 1/4 inch. Sealing of joints will not be required.

Surface

The surface of the concrete shall be floated to a level uniform surface and left with a slightly rounded surface. The surface shall be roughened with mechanic's brush to prevent smooth and slippery surfaces. No surface shall be troweled to a glassy finish. Edges at the forms and joints shall be rounded with an edging tool.

Curb Ramps and Approaches

New roadway construction in locations where non-motorized facilities are anticipated shall incorporate concrete curbing of RCKC standards at intersections. Roadway curbing shall be extended through the roadway intersection, within the right-of-way limits of each approaching roadway. The purpose of this standard is to ensure that all non-motorized ramps, (current and anticipated) are fully integrated into the casting of the concrete curb through the intersection. As required, concrete curbing within intersections shall be extended a sufficient length to assure that the concrete curb carries the transition from the existing edge of pavement design of each approaching roadway. Commonly encountered edge-of-roadway pavement designs include ditch drainage, gravel shoulder, bituminous valley gutter, mountable and high back concrete curb.

Curb ramps shall be concrete unless written approval of an alternative is pre-approved by the Township. In locations where existing concrete curb is without an integrated approach (curb-cut), the entire roadway concrete curb section may need to be removed and re-constructed to meet ADA compliance standards. On a case by case basis, the removal of an existing curb back by saw cutting may be able to yield an ADA compliant transition through the curb. Proposals to use the saw cut method of ramp construction should seek prior approval from the Township.

Ramp that approach existing roads without concrete curbing should be reviewed with the Township prior to ramp construction. If providing new concrete curb for the ramp transition is determined to be impracticable, or if extensive re-construction the intersection curb is determined to be excessive, ADA compliant asphalt transitions onto asphalt paving may be accepted upon prior written approval of the township.

Butt joints must be provided at the transition from concrete to bituminous paving for all non-motorized facilities. Saw cutting, grinding, or similar means of joint formation is required. As practicable, transition butt-joints to asphalt curbing shall be located at the curb high-point. Material transition joints are not allowed along a drainage flow-line. Surface grinding of existing asphalt curbing is an acceptable means to achieve ADA compliant transitions. Alternately, IR asphalt restoration or placement of a new transition segment of asphalt paving can be constructed through the curb profile to achieve the transition from the asphalt roadway to the concrete ramp. Regardless

of construction method or prior approval of the means of construction, compliance with ADA standards will need to be demonstrated to receive acceptance of the work.

Sidewalk Detectable Warnings

Sidewalk Detectable Warning shall apply to all construction or reconstruction of streets, curbs, or sidewalks. Detectable Warnings shall be placed at all street intersections and at select commercial/industrial driveways where driveway traffic and pedestrian crossings are determined/projected by the Township to generate sufficient conflicts to warrant the greater risk awareness provided by detectable warnings.

Detectable Warnings shall conform to ADA requirements, MDOT Specifications and MDOT Detail R-28-E. Detectable Warnings shall be made of ductile iron material, extend the full width of the curb/sidewalk ramp. They shall be located so that the edge nearest the curb line or street is 6" to 8" from the curb line.

HOT MIX ASPHALT SHARED-USE PATH CONSTRUCTION

Shared-use paths shall be a minimum of ten (10) feet wide unless a different width is required by other Township ordinances or regulations.

Grade

The sidewalk shall be constructed to match the existing grade, or as noted on construction drawings. The path will have a transverse slope either toward or away from the road to maintain existing drainage patterns. Minor fills and cuts will be made in the field during construction to provide smooth transition of the path and maintain existing drainage patterns.

Sub-base Preparation

Existing vegetation shall be removed and topsoil excavated to provide a six (6) inch sand sub-base for the proposed path. The existing sand sub-base shall be compacted to ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures. Where fill sand is required it shall be compacted to achieve ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures.

Gravel Base

The gravel base shall extend one foot beyond the width of the topcoat on each side and consist of six (6) inches of compacted MDOT 22A gravel in accordance with the section as indicated on the construction drawing. Density of the gravel shall be ninety five (95%) percent.

Hot Mix Asphalt (HMA) Surface

The hot mix asphalt surface shall consist of 120 lbs/SY MDOT 36A hot mix asphalt (top) over 165 lbs/SY of MDOT 13A hot mix asphalt (level) placed in accordance with the section as indicated on

the construction drawings. The asphalt performance grade shall be 58-28. The hot mix asphalt shall be placed with a self-propelled paver; spreader boxes will not be permitted. A bond coat shall be applied between successive courses of hot mix asphalt at an application rate of 0.1 gal./SY.

REMOVABLE BOLLARDS (Vehicle Exclusion Device)

Certain Shared-Use Pathways may include removable Bollards as vehicle exclusion devices. When shown on plans, this work shall include the construction of the concrete footings, in-ground receiver, installation of the bollards, and all related work necessary to provide complete Removable Bollards as shown on the plans.

Materials

The materials shall be as specified below:

1. Removable Bollard: Model R-7901 Steel Removable Bollard with Removable Mounting and locking hinged steel lid from Reliance Foundry, Petersen Mfg. Co., Inc., or approved equal. Bollard shall be corrosion resistant under coating and safety yellow in color with safety yellow reflective tape at the top.
2. Concrete: Provide grade S1 concrete as specified in Section 701 of the MDOT Standard Specifications for Construction.
3. Steel Reinforcement: Provide steel reinforcement as specified in Section 905 of the MDOT Standard Specifications for Construction.
4. Submittals: The Contractor shall submit complete shop drawings, showing all materials, dimension, fastenings, and fastening devices.

Construction

The Removable Bollards shall be constructed according to the plans and as described below.

1. Preparation work shall be thoroughly reviewed with the Engineer prior to installation. Saw cut the existing HMA prior to placement.
2. Install Removable Bollards complete per manufacturer's specifications and instructions and as detailed on approved shop drawings.
3. Concrete: Install concrete components as specified in Section 706 of the MDOT Standard Specifications for Construction and as detailed on the plans.
4. All excess and waste materials shall be disposed of legally off-site. Upon completion of the work, leave areas in a clean condition.

SIGNS

Certain Shared-Use Pathways may include restricted usage or way-finding signs. When shown on plans, this work shall comply with the construction standards of the Kalamazoo County Road Commission and all permit requirements of the applicable operating authority of the public right-of-way. In order to promote uniformity in style (community sense-of-place), shop drawings for all signage shall be submitted for Oshtemo Township approval prior to installation.

LAWN AND YARD RESTORATION

After construction is completed all disturbed lawn areas, including adjacent cut and fill areas as required to blend into the existing yards, shall be repaired using a maximum of 1 on 4 back slope, four (4) inches of topsoil, MDOT Class A seed, fertilizer and mulch.

All disturbed areas, including adjacent cut and fill areas as required to blend into the existing yards, which are not lawns, shall be repaired using a maximum of 1 on 3 back slope, two (2) inches of topsoil, MDOT Roadside Seed, fertilizer and mulch.

Restored areas shall be repaired and reseeded as often as necessary in order to produce a close stand of weed free grass to the edges of the sidewalk or multi-use path.

ATTACHMENTS:

MDOT Construction Details (2013 or current edition):

R-28-H Sidewalk Ramp and Detectable Warning Details

R-29-H Driveway Openings & Approaches and Concrete Sidewalk

P&N Slope Cut & Fill Limitations

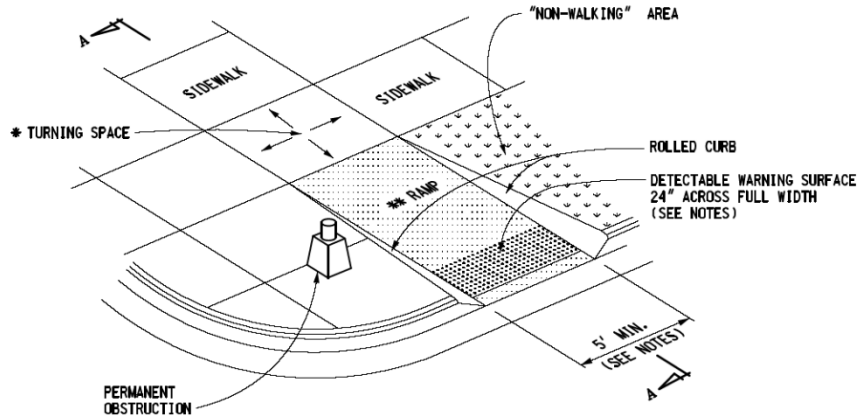
P&N Precast Concrete Rip-Rap Retaining Wall

P&N Lumber Retaining Wall

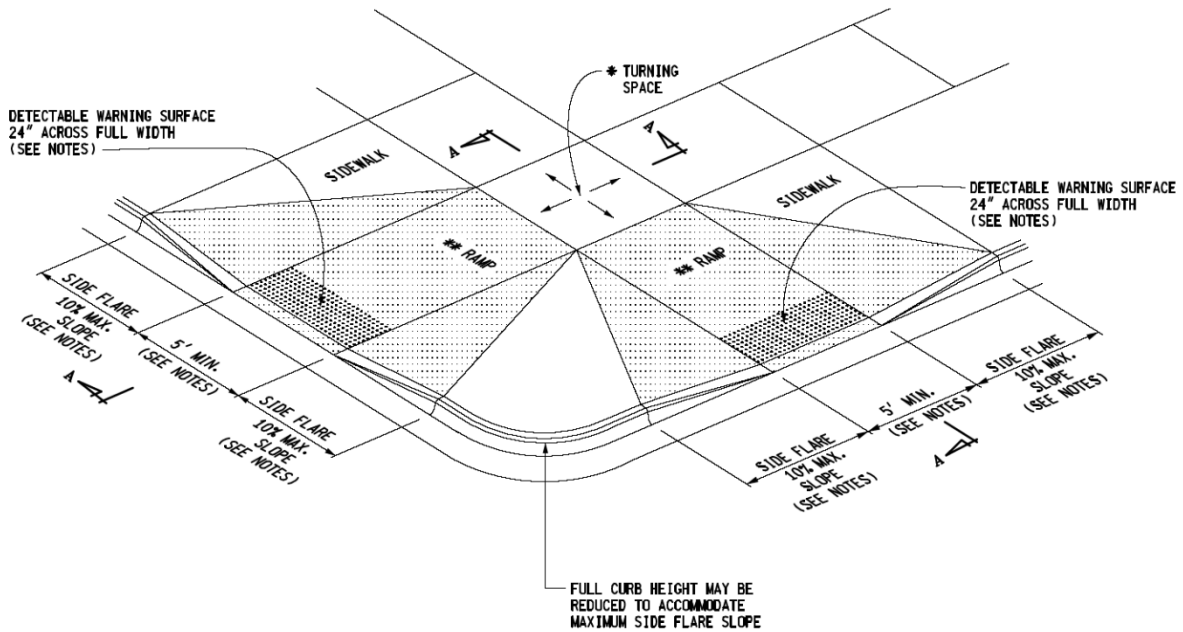
P&N Lumber Fencing

* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.



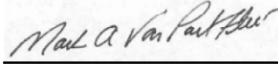
** MAXIMUM RAMP CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



SIDEWALK RAMP TYPE R
(ROLLED SIDES)

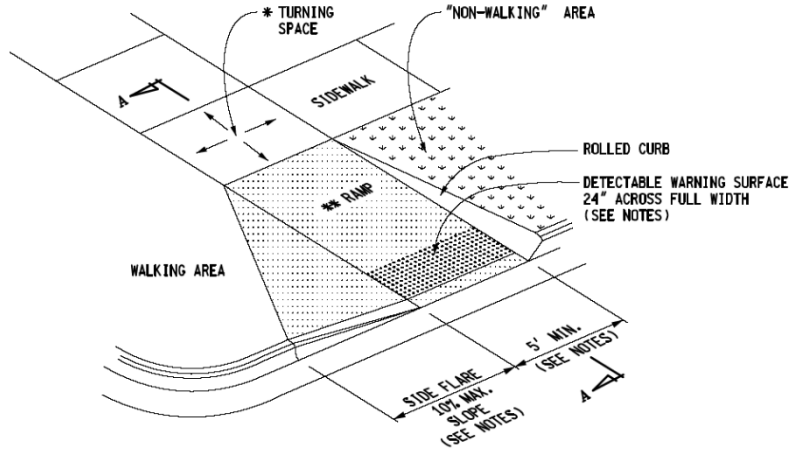


SIDEWALK RAMP TYPE F
(FLARED SIDES, TWO RAMPS SHOWN)

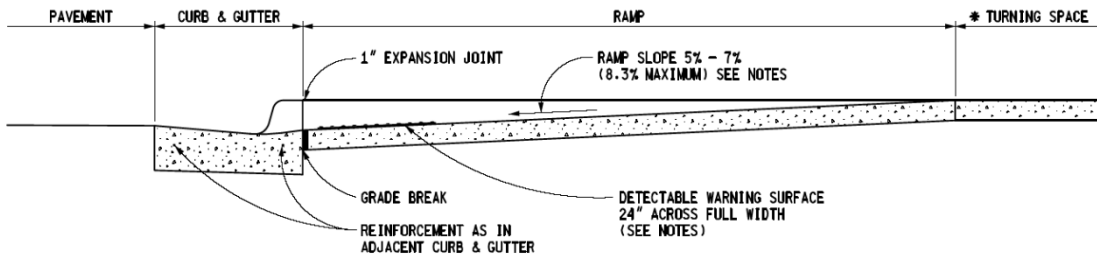
 PREPARED BY DESIGN DIVISION DRAWN BY: <u>B.L.T.</u> CHECKED BY: <u>W.K.P.</u>	DEPARTMENT DIRECTOR Kirk T. Stuedle APPROVED BY:  DIRECTOR, BUREAU OF FIELD SERVICES	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR SIDEWALK RAMP AND DETECTABLE WARNING DETAILS		
	APPROVED BY:  DIRECTOR, BUREAU OF HIGHWAY DEVELOPMENT	1-25-2013 F.H.W.A. APPROVAL	10-3-2012 PLAN DATE	R-28-H SHEET 1 OF 7

* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.

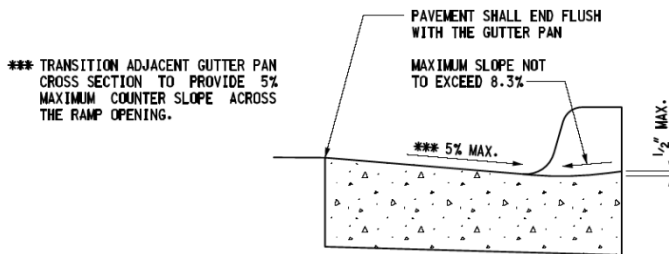
** MAXIMUM RAMP CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



SIDEWALK RAMP TYPE RF
(ROLLED / FLARED SIDES)



SECTION A-A



SECTION THROUGH CURB CUT
(TYPICAL ALL RAMP TYPES)

MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND
DETECTABLE WARNING DETAILS**

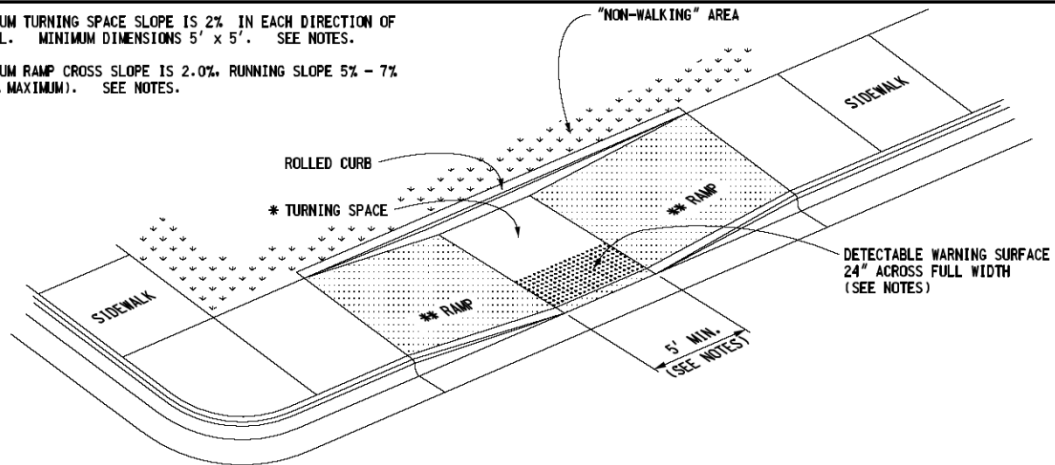
1-25-2013
F.H.W.A. APPROVAL

10-3-2012
PLAN DATE

R-28-H

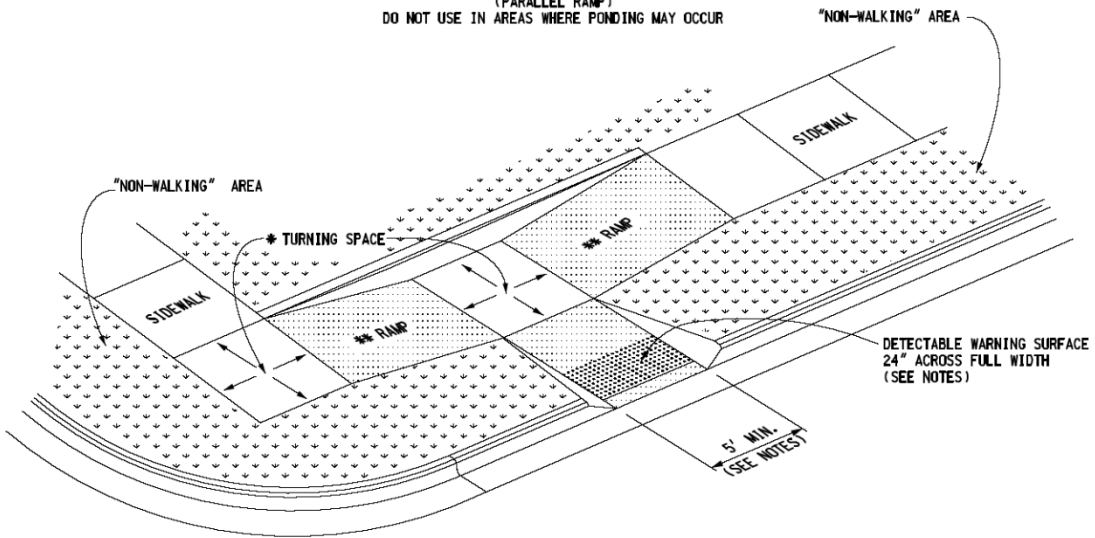
SHEET
2 OF 7

* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.
 ** MAXIMUM RAMP CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



SIDEWALK RAMP TYPE P

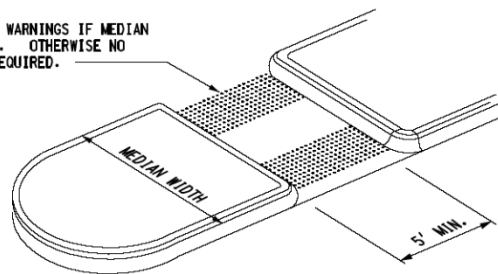
(PARALLEL RAMP)
 DO NOT USE IN AREAS WHERE PONDING MAY OCCUR



SIDEWALK RAMP TYPE C

(COMBINATION RAMP)

USE 24" DEEP DETECTABLE WARNINGS IF MEDIAN WIDTH IS AT LEAST 6'-0". OTHERWISE NO DETECTABLE WARNING IS REQUIRED.



SIDEWALK RAMP TYPE M

(MEDIAN ISLAND)

MICHIGAN DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND
 DETECTABLE WARNING DETAILS**

1-25-2013
 F.H.W.A. APPROVAL

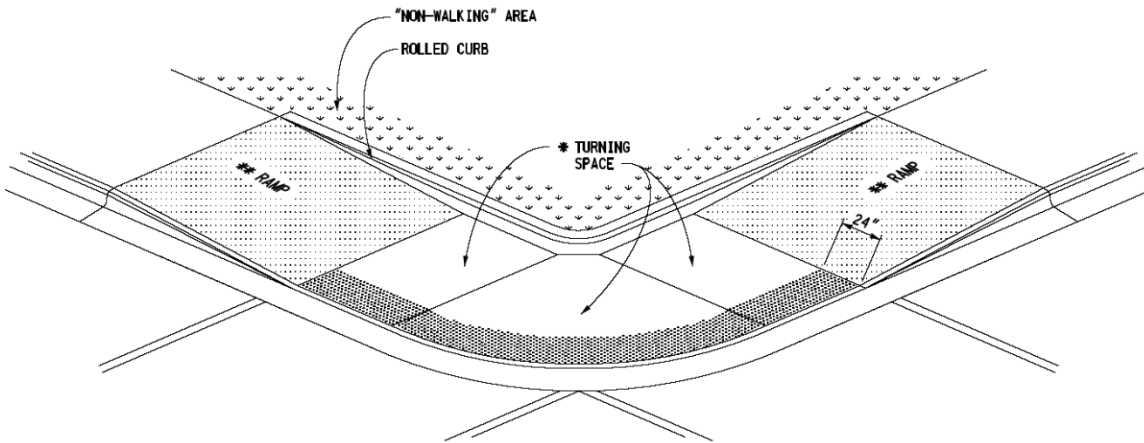
10-3-2012
 PLAN DATE

R-28-H

SHEET
 3 OF 7

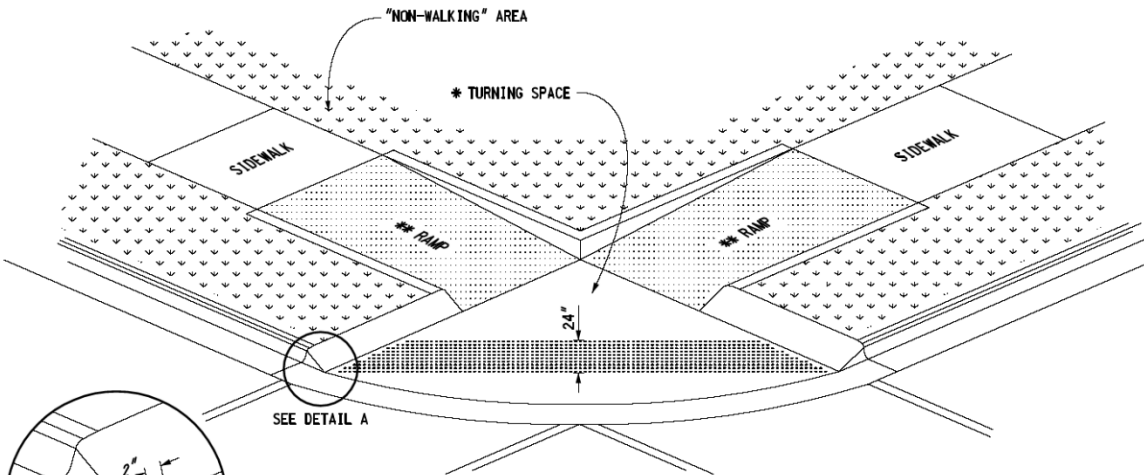
* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.

** MAXIMUM RAMP CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



(RADIAL DETECTABLE WARNING SHOWN)

DETECTABLE WARNING SURFACE COVERAGE IS 24" FOR THE FULL WIDTH OF THE RAMP OPENING EXCLUDING CURBED OR CURB TRANSITION AREAS. A CURB OFFSET NOT GREATER THAN 2" MEASURED ALONG THE EDGES OF THE DETECTABLE WARNING IS ALLOWABLE.



DETAIL A

(TANGENT DETECTABLE WARNING SHOWN)

A CONCRETE BORDER OFFSET NOT GREATER THAN 2" IS ALLOWABLE FOR DETECTABLE WARNING INSTALLATION WHERE THE BACK OF CURB IS ON A RADIUS, THE BORDER OFFSET MAY BE MEASURED FROM THE END OF THE RADIUS.

SIDEWALK RAMP TYPE D

(DEPRESSED CORNER)

USE ONLY WHEN INDEPENDENT DIRECTIONAL RAMPS CAN NOT BE CONSTRUCTED FOR EACH CROSSING DIRECTION

MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND
DETECTABLE WARNING DETAILS**

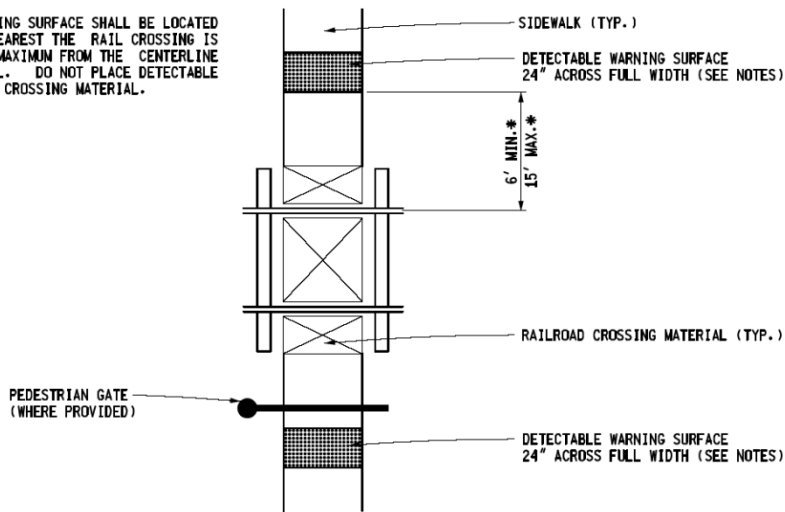
1-25-2013
F.H.W.A. APPROVAL

10-3-2012
PLAN DATE

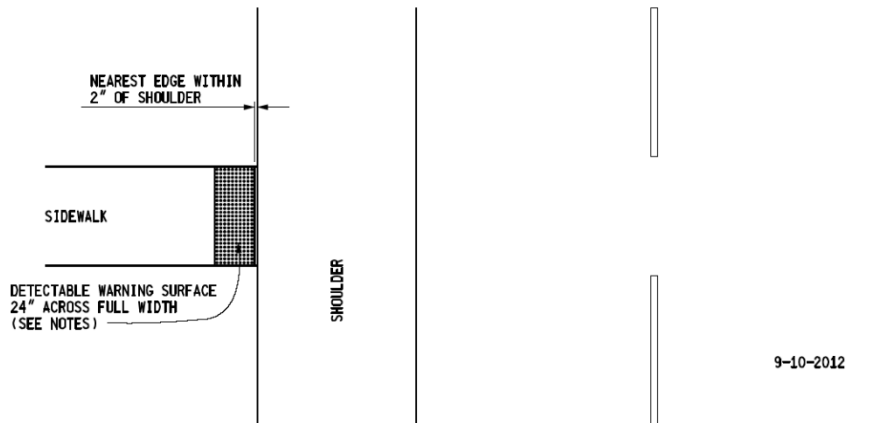
R-28-H

SHEET
4 OF 7

* THE DETECTABLE WARNING SURFACE SHALL BE LOCATED SO THAT THE EDGE NEAREST THE RAIL CROSSING IS 6' MINIMUM AND 15' MAXIMUM FROM THE CENTERLINE OF THE NEAREST RAIL. DO NOT PLACE DETECTABLE WARNING ON RAILROAD CROSSING MATERIAL.

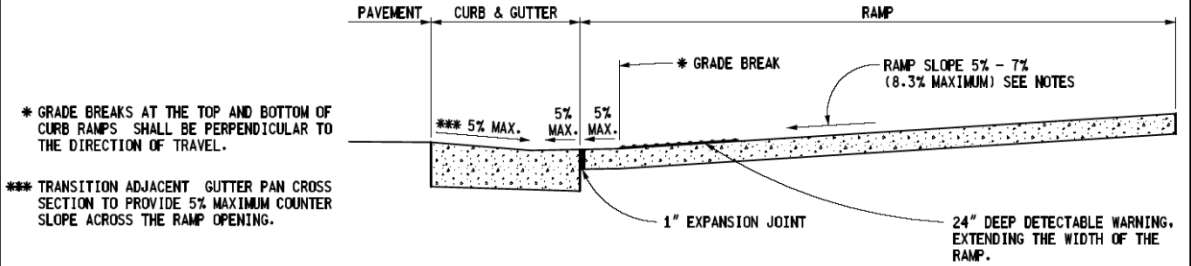
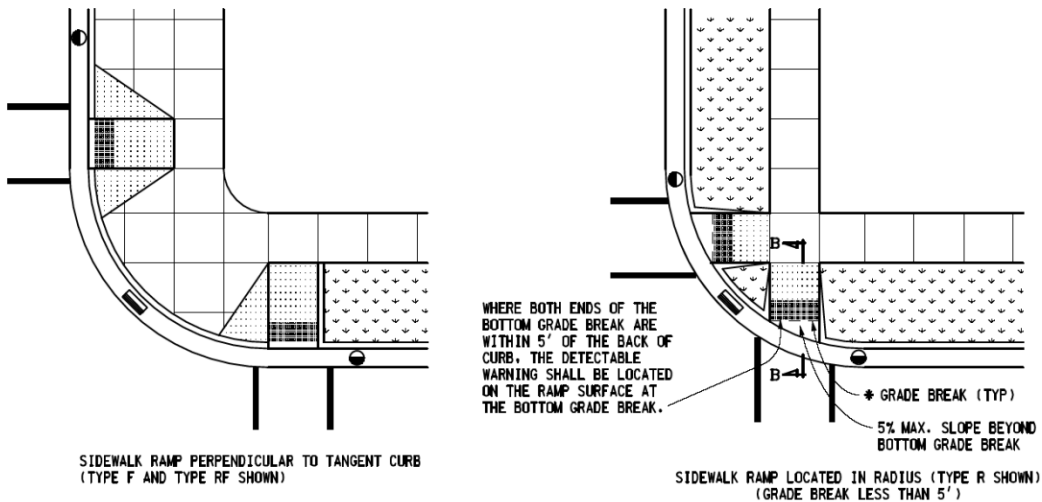
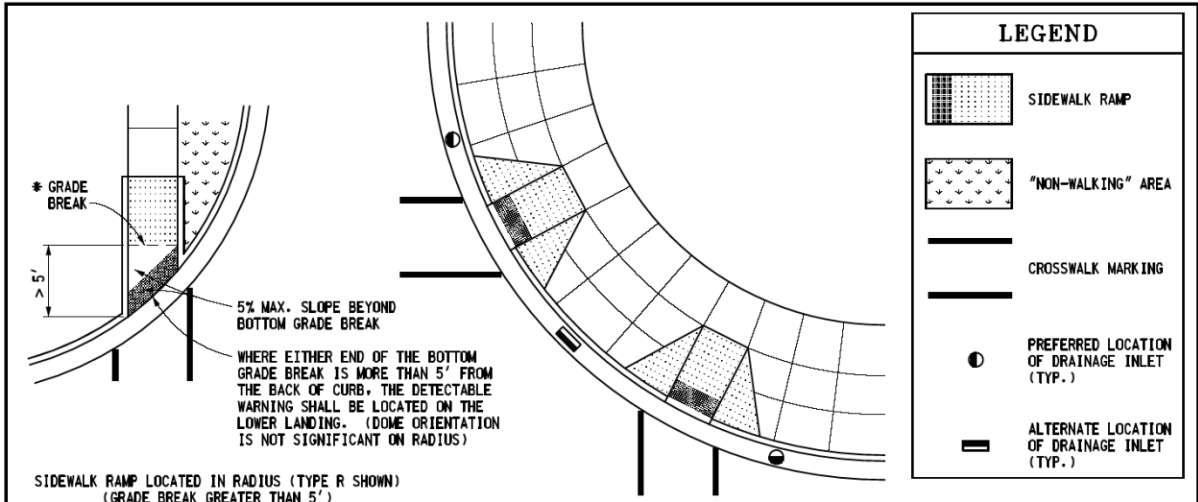


DETECTABLE WARNING AT RAILROAD CROSSING



DETECTABLE WARNING AT FLUSH SHOULDER OR ROADWAY

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR SIDEWALK RAMP AND DETECTABLE WARNING DETAILS			
1-25-2013 F.H.W.A. APPROVAL	10-3-2012 PLAN DATE	R-28-H	SHEET 5 OF 7

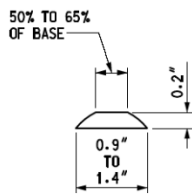


**SECTION B-B
SIDEWALK RAMP ORIENTATION**

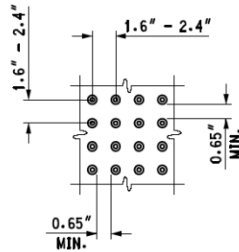
MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND
DETECTABLE WARNING DETAILS**

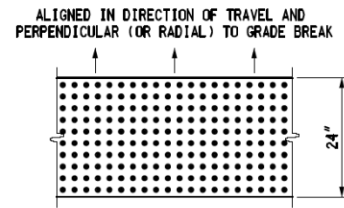
1-25-2013 F.H.W.A. APPROVAL	10-3-2012 PLAN DATE	R-28-H	SHEET 6 OF 7
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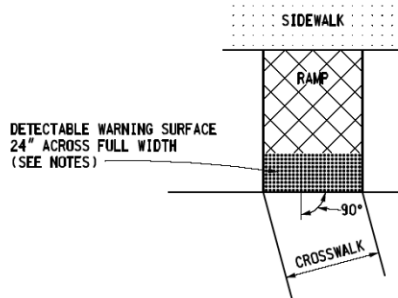
DOME SECTION



DOME SPACING



DOME ALIGNMENT



DETECTABLE WARNING DETAILS

NOTES:

DETAILS SPECIFIED ON THIS PLAN APPLY TO ALL CONSTRUCTION, RECONSTRUCTION, OR ALTERATION OF STREETS, CURBS, OR SIDEWALKS BY ALL PUBLIC AGENCIES AND BY ALL PRIVATE ORGANIZATIONS CONSTRUCTING FACILITIES FOR PUBLIC USE.

SIDEWALK RAMPS ARE TO BE LOCATED AS SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

RAMPS SHALL BE PROVIDED AT ALL CORNERS OF AN INTERSECTION WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB. RAMPS SHALL ALSO BE PROVIDED AT MARKED AND/OR SIGNALIZED MID-BLOCK CROSSINGS.

SURFACE TEXTURE OF THE RAMP SHALL BE THAT OBTAINED BY A COARSE BROOMING, TRANSVERSE TO THE RUNNING SLOPE.

SIDEWALK SHALL BE RAMPED WHERE THE DRIVEWAY CURB IS EXTENDED ACROSS THE WALK.

CARE SHALL BE TAKEN TO ASSURE A UNIFORM GRADE ON THE RAMP. WHERE CONDITIONS PERMIT, IT IS DESIRABLE THAT THE SLOPE OF THE RAMP BE IN ONLY ONE DIRECTION, PARALLEL TO THE DIRECTION OF TRAVEL.

RAMP WIDTH SHALL BE INCREASED, IF NECESSARY, TO ACCOMMODATE SIDEWALK SNOW REMOVAL EQUIPMENT NORMALLY USED BY THE MUNICIPALITY.

PROVIDE TURNING SPACES WHERE PEDESTRIAN TURNING MOVEMENTS ARE REQUIRED.

WHEN 5' MINIMUM WIDTHS ARE NOT FEASIBLE, RAMP WIDTH MAY BE REDUCED TO NOT LESS THAN 4' AND TURNING SPACES TO NOT LESS THAN 4' x 4'.

DETECTABLE WARNING SURFACE COVERAGE IS 24" MINIMUM IN THE DIRECTION OF RAMP/PATH TRAVEL AND THE FULL WIDTH OF THE RAMP/PATH OPENING EXCLUDING CURBED OR FLARED CURB TRANSITION AREAS. A CURB OFFSET NOT GREATER THAN 2" MEASURED ALONG THE EDGES OF THE DETECTABLE WARNING (OR AS OTHERWISE SHOWN ON THIS STANDARD) IS ALLOWABLE.

FOR NEW ROADWAY CONSTRUCTION, THE RAMP CROSS SLOPE MAY NOT EXCEED 2%. FOR ALTERATIONS TO EXISTING ROADWAYS, THE CROSS SLOPE MAY BE TRANSITIONED TO MEET AN EXISTING ROADWAY GRADE. THE CROSS SLOPE TRANSITION SHALL BE APPLIED UNIFORMLY OVER THE FULL LENGTH OF THE RAMP.

THE MAXIMUM RUNNING SLOPE OF 8.3% IS RELATIVE TO A FLAT (0%) REFERENCE. HOWEVER, IT SHALL NOT REQUIRE ANY RAMP OR SERIES OF RAMPS TO EXCEED 15 FEET IN LENGTH.

DRAINAGE STRUCTURES SHOULD NOT BE PLACED IN LINE WITH RAMPS. THE LOCATION OF THE RAMP SHOULD TAKE PRECEDENCE OVER THE LOCATION OF THE DRAINAGE STRUCTURE. WHERE EXISTING DRAINAGE STRUCTURES ARE LOCATED IN THE RAMP PATH OF TRAVEL, USE A MANUFACTURER'S ADA COMPLIANT GRATE. OPENINGS SHALL NOT BE GREATER THAN 1/2". ELONGATED OPENINGS SHALL BE PLACED SO THAT THE LONG DIMENSION IS PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL.

TRANSITION THE GUTTER PAN CROSS SECTION SUCH THAT THE COUNTER SLOPE IN THE DIRECTION OF RAMP TRAVEL IS NOT GREATER THAN 5%. MAINTAIN THE NORMAL GUTTER PAN CROSS SECTION ACROSS DRAINAGE STRUCTURES.

THE TOP OF THE JOINT FILLER FOR ALL RAMP TYPES SHALL BE FLUSH WITH THE ADJACENT CONCRETE.

CROSSWALK AND STOP LINE MARKINGS, IF USED, SHALL BE SO LOCATED AS TO STOP TRAFFIC SHORT OF RAMP CROSSINGS. SPECIFIC DETAILS FOR MARKING APPLICATIONS ARE GIVEN IN THE "MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".

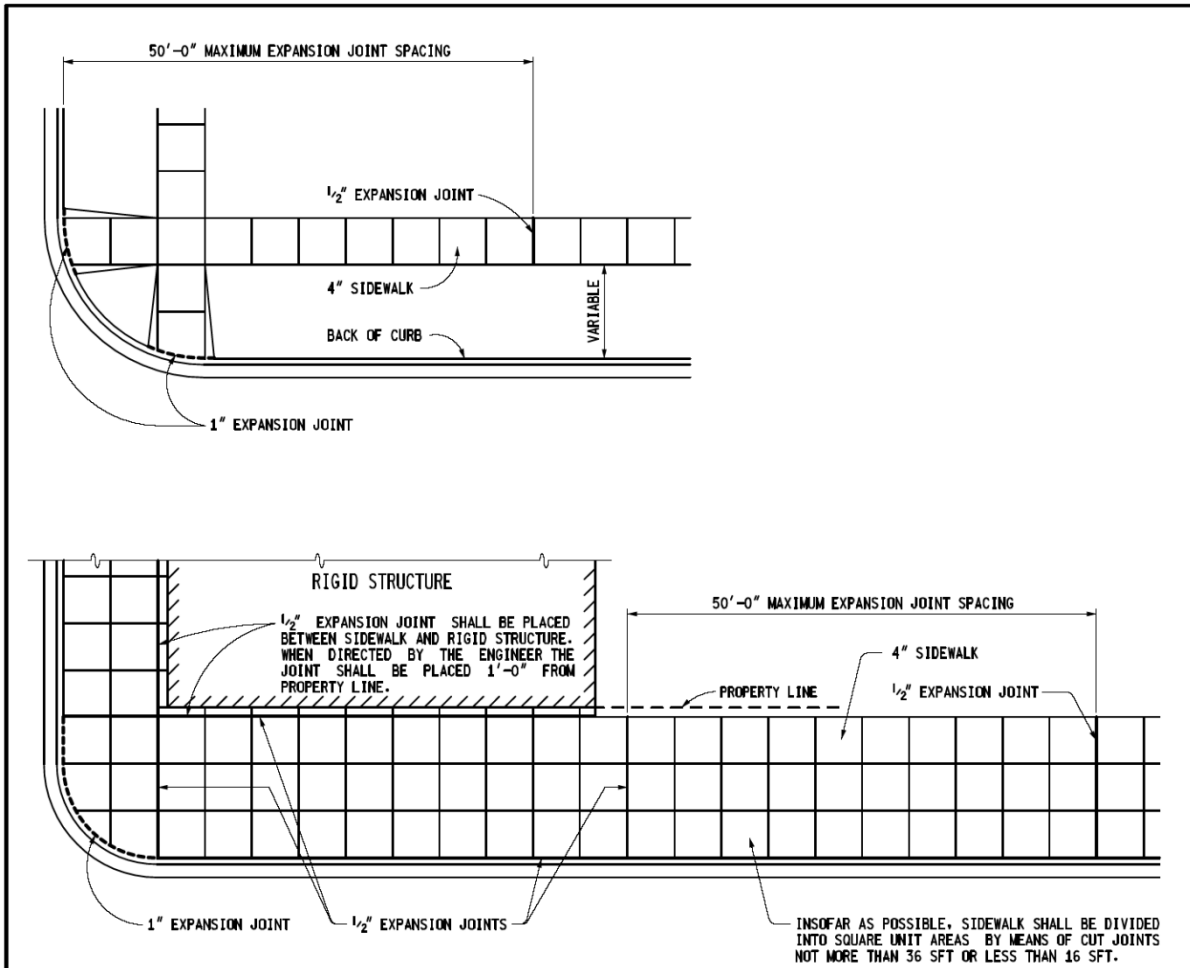
FLARED SIDES WITH A SLOPE OF 10% MAXIMUM, MEASURED ALONG THE ROADSIDE CURB LINE, SHALL BE PROVIDED WHERE AN UNOBSTRUCTED CIRCULATION PATH LATERALLY CROSSES THE SIDEWALK RAMP. FLARED SIDES ARE NOT REQUIRED WHERE THE RAMP IS BORDERED BY LANDSCAPING, UNPAVED SURFACE OR PERMANENT FIXED OBJECTS. WHERE THEY ARE NOT REQUIRED, FLARED SIDES CAN BE CONSIDERED IN ORDER TO AVOID SHARP CURB RETURNS AT RAMP OPENINGS.

DETECTABLE WARNING PLATES MUST BE INSTALLED USING FABRICATED OR FIELD CUT UNITS CAST AND/OR ANCHORED IN THE PAVEMENT TO RESIST SHIFTING OR HEAVING.

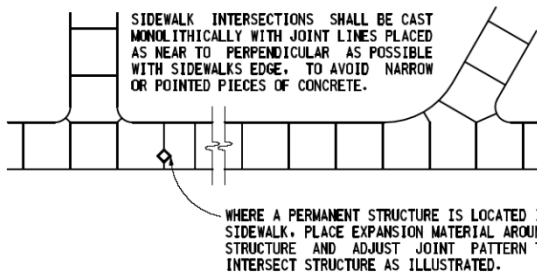
MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND
DETECTABLE WARNING DETAILS**

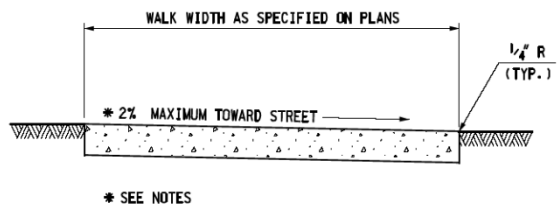
1-25-2013 F.H.W.A. APPROVAL	10-3-2012 PLAN DATE	R-28-H	SHEET 7 OF 7
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LOCATION OF JOINTS IN CONCRETE SIDEWALK

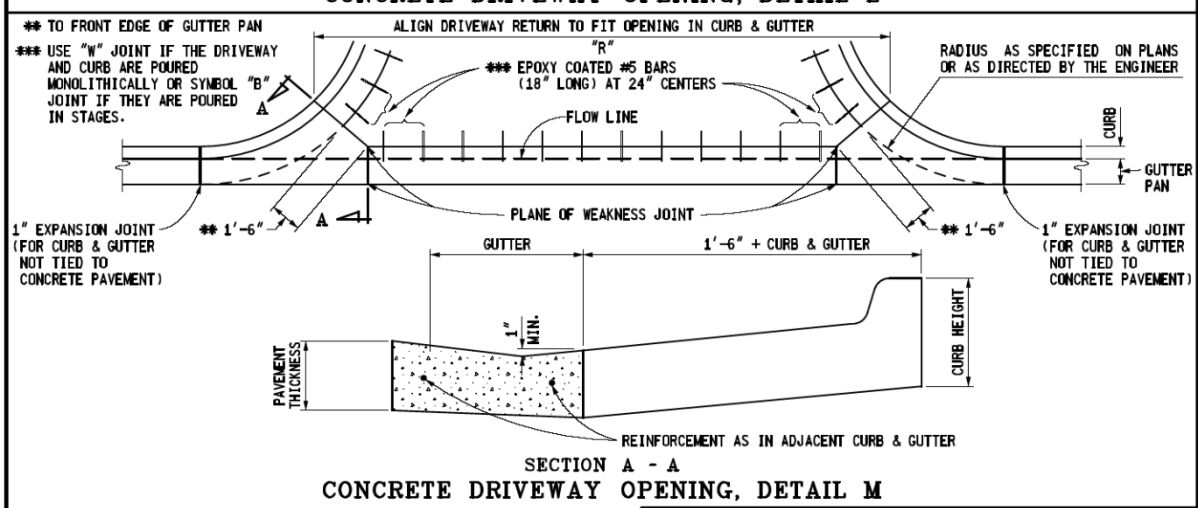
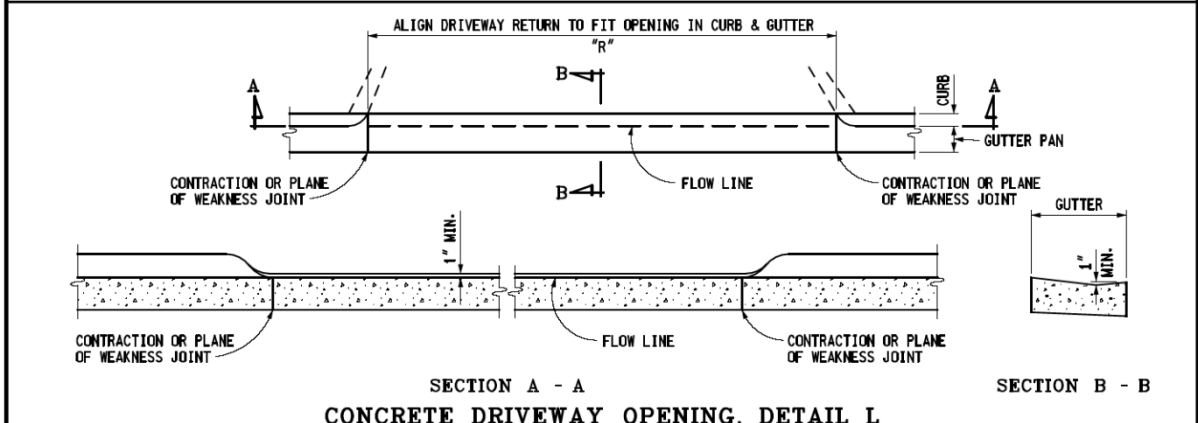
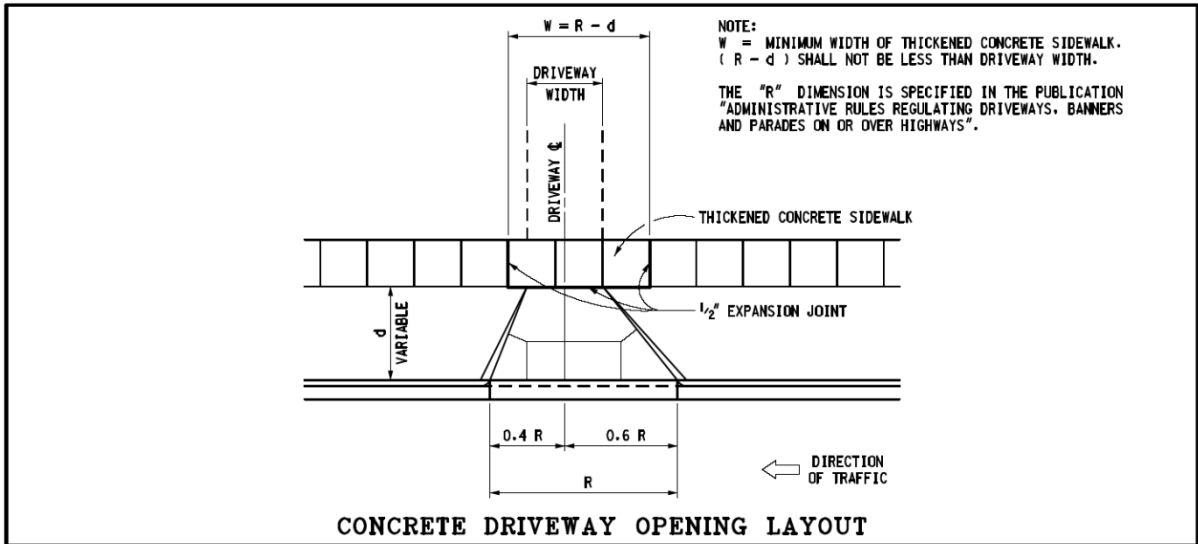


TYPICAL SIDEWALK JOINT LAYOUTS



4" CONCRETE SIDEWALK

<p>PREPARED BY DESIGN DIVISION</p> <p>DRAWN BY: <u>B.L.T.</u></p> <p>CHECKED BY: <u>W.K.P.</u></p>	<p>DEPARTMENT DIRECTOR Kirk T. Steudle</p> <p>APPROVED BY: <i>Randy V. Pittel</i> DIRECTOR, BUREAU OF FIELD SERVICES</p>	<p>MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR</p> <p>DRIVEWAY OPENINGS & APPROACHES, AND CONCRETE SIDEWALK</p>		
	<p>APPROVED BY: <i>Mark A. Van Pelt</i> DIRECTOR, BUREAU OF HIGHWAY DEVELOPMENT</p>	<p>1-25-2013 F.H.W.A. APPROVAL</p>	<p>10-1-2012 PLAN DATE</p>	<p>R-29-H</p>

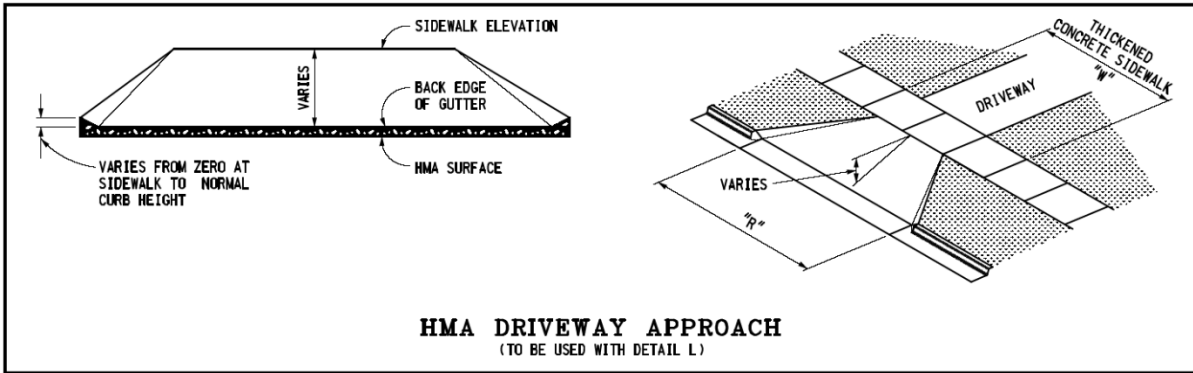


NOTE:
 FOR ROADWAYS WITH CONCRETE PAVEMENTS, LONGITUDINAL LANE TIES WILL BE CONTINUOUS THROUGH THE DRIVEWAY OPENING AND THE SPACING OF THE #5 BARS IN CONCRETE DRIVEWAYS SHALL BE ADJUSTED TO AVOID CONFLICT WITH THE LONGITUDINAL LANE TIES.

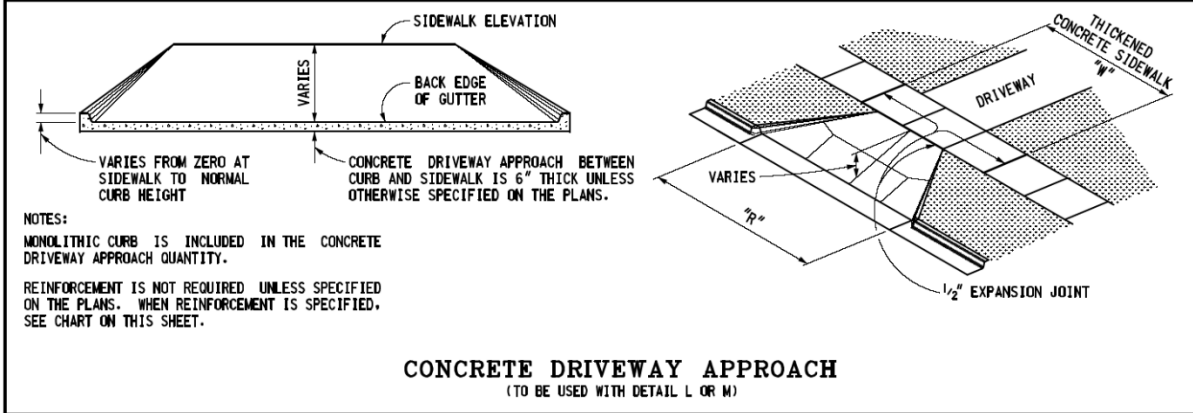
MICHIGAN DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**DRIVEWAY OPENINGS
 & APPROACHES,
 AND CONCRETE SIDEWALK**

1-25-2013 F.H.W.A. APPROVAL	10-1-2012 PLAN DATE	R-29-H	SHEET 2 OF 4
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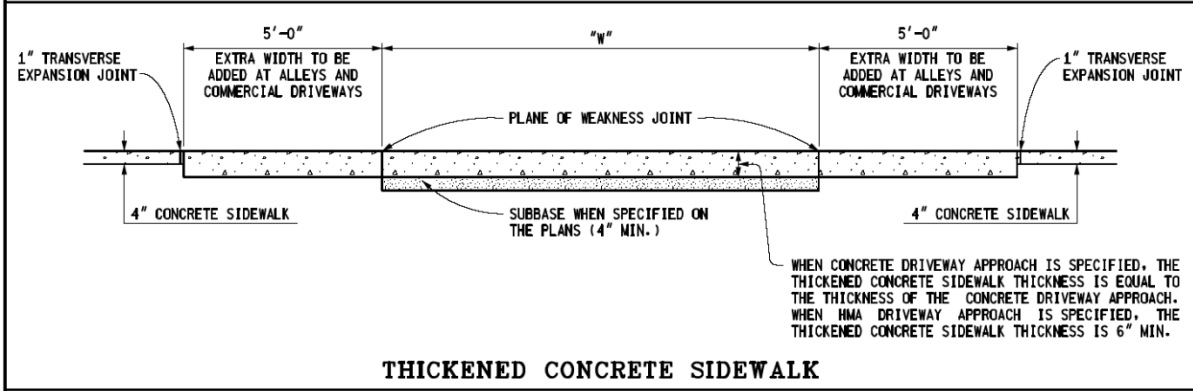


HMA DRIVEWAY APPROACH
(TO BE USED WITH DETAIL L)

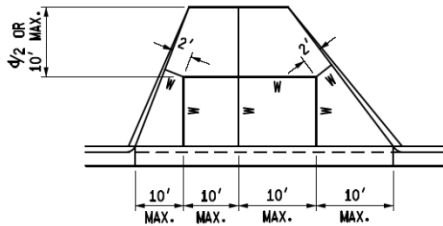


CONCRETE DRIVEWAY APPROACH
(TO BE USED WITH DETAIL L OR M)

NOTES:
MONOLITHIC CURB IS INCLUDED IN THE CONCRETE DRIVEWAY APPROACH QUANTITY.
REINFORCEMENT IS NOT REQUIRED UNLESS SPECIFIED ON THE PLANS. WHEN REINFORCEMENT IS SPECIFIED, SEE CHART ON THIS SHEET.



THICKENED CONCRETE SIDEWALK



ADJUST DRIVEWAY JOINTS AS NEEDED TO ALIGN WITH ANY COINCIDING TRANSVERSE PAVEMENT JOINTS.
JOINT LAYOUT IS AS INDICATED OR AS DIRECTED BY THE ENGINEER.

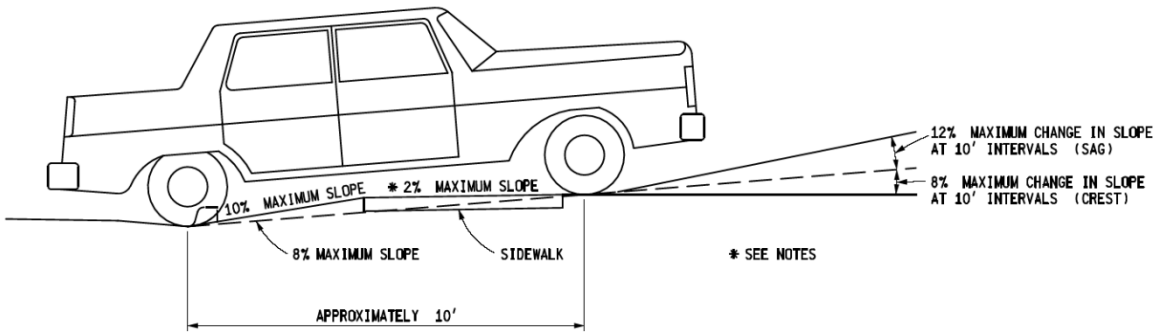
INTERMEDIATE DRIVEWAY JOINT DETAILS

REINFORCEMENT FOR CONCRETE DRIVEWAYS		
CONCRETE DRIVEWAY THICKNESS	WIRE SIZE (6" x 6" MESH)	AVERAGE WEIGHT (LBS/100 SFT)
LESS THAN 8"	W1.4	21
	W2.9	42
8" OR GREATER	USE WIRE FABRIC REINFORCEMENT SPECIFIED ON STANDARD PLAN R-37-SERIES	

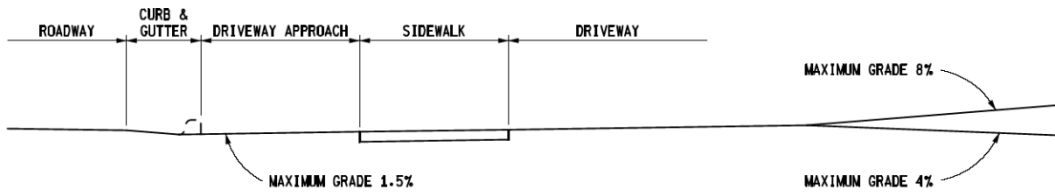
MICHIGAN DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**DRIVEWAY OPENINGS
& APPROACHES,
AND CONCRETE SIDEWALK**

1-25-2013 F.H.W.A. APPROVAL	10-1-2012 PLAN DATE	R-29-H	SHEET 3 OF 4
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LOW VOLUME COMMERCIAL OR RESIDENTIAL DRIVEWAY SLOPES



COMMERCIAL DRIVEWAY PROFILE FOR MAJOR TRAFFIC GENERATORS

NOTES:

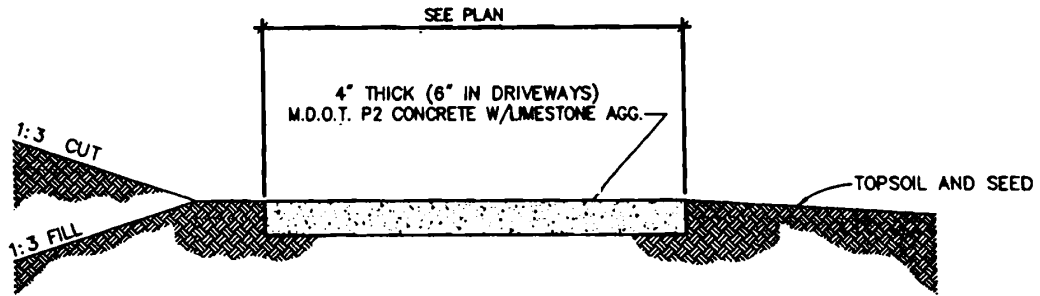
FOR DRIVEWAY DESIGN REFER ALSO TO "ADMINISTRATIVE RULES REGULATING DRIVEWAYS, BANNERS, AND PARADES ON OR OVER HIGHWAYS" AND GEOMETRIC DESIGN G-680-SERIES, COMMERCIAL DRIVEWAYS.

FOR CURB AND GUTTER DETAILS, SEE STANDARD PLAN R-30-SERIES.

TRANSVERSE SIDEWALK SLOPES ARE TYPICALLY 1.5% OR 2% MAXIMUM. IN ORDER TO MEET SITE CONDITIONS, IF THE TRANSVERSE SLOPE IS REQUIRED TO BE LESS THAN 1.5%, LONGITUDINAL DRAINAGE MUST BE PROVIDED.

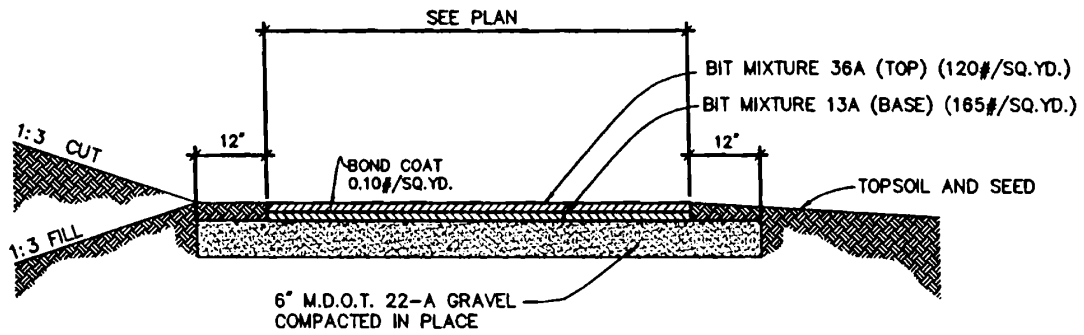
WHEN SETTING GRADES FOR COMMERCIAL DRIVES, THE TYPES OF VEHICLES USING THE DRIVE SHOULD BE CONSIDERED.

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR DRIVEWAY OPENINGS & APPROACHES, AND CONCRETE SIDEWALK			
1-25-2013 F.H.W.A. APPROVAL	10-1-2012 PLAN DATE	R-29-H	SHEET 4 OF 4



CONCRETE SIDEWALK DETAIL

NO SCALE

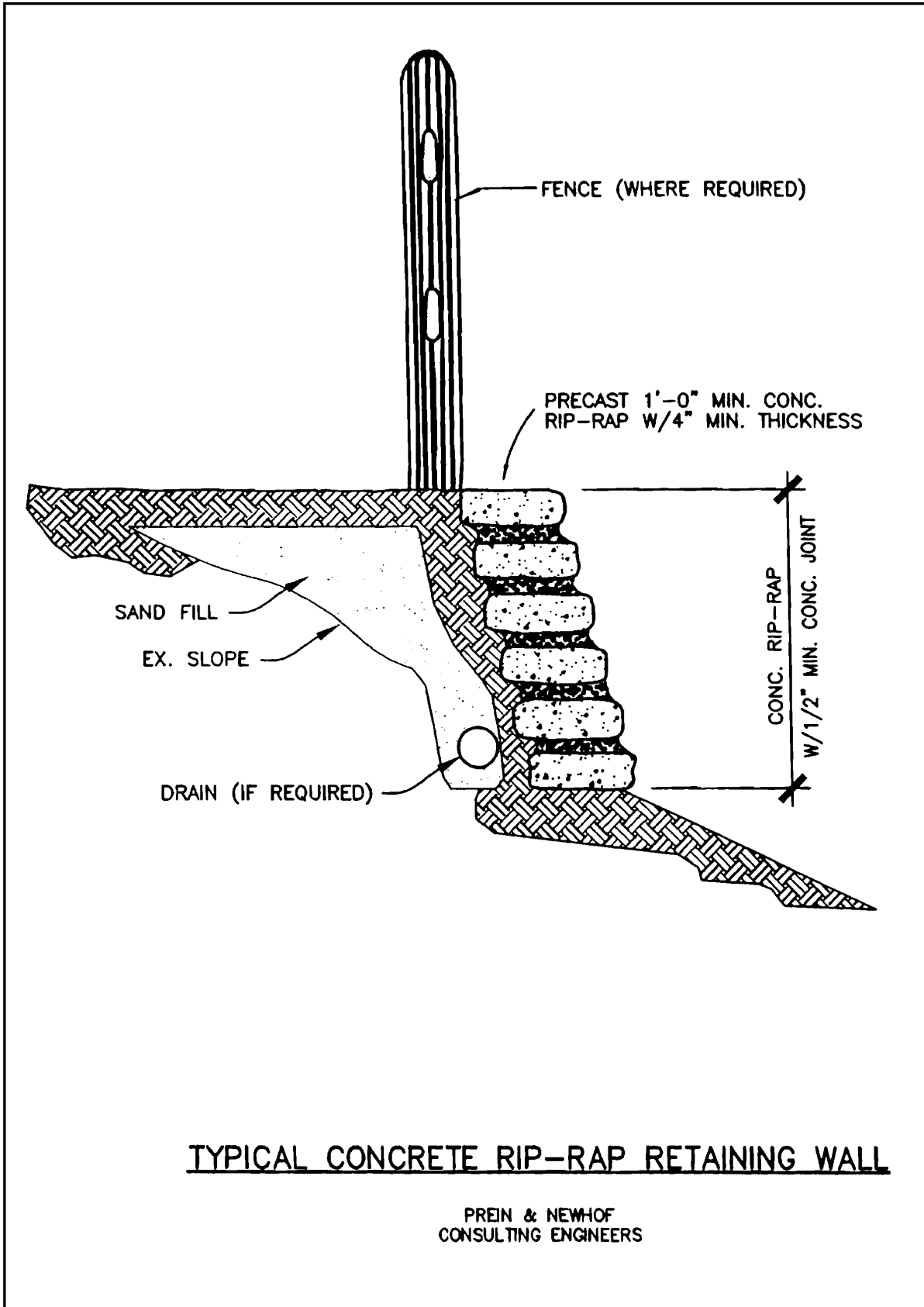


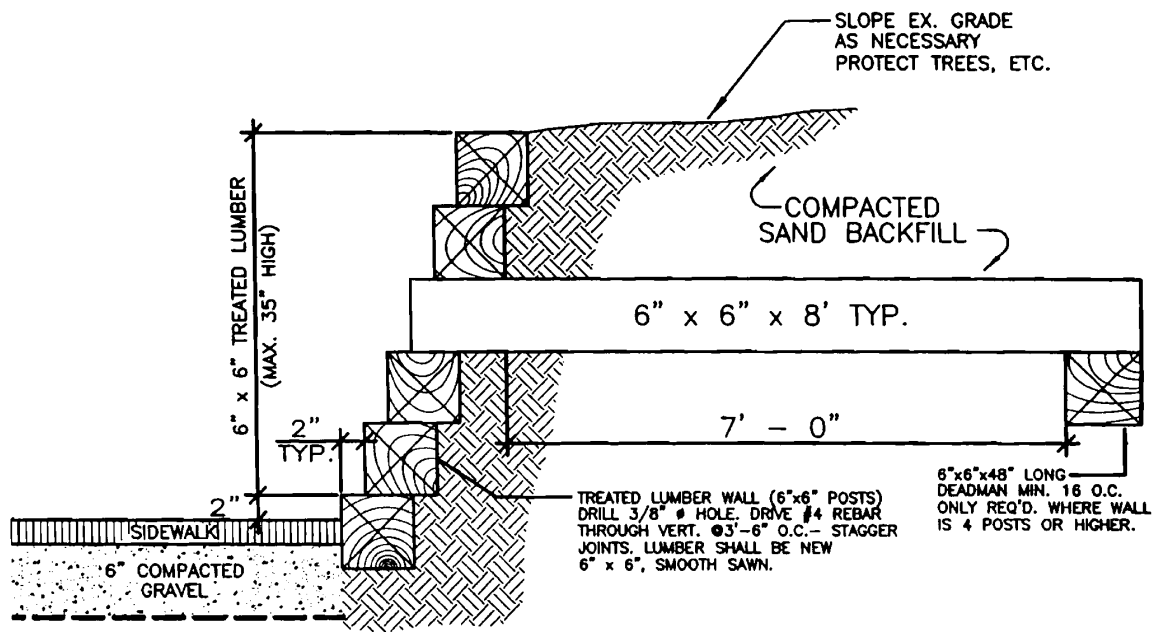
BITUMINOUS MULTI-PURPOSE PATH DETAIL

NO SCALE

SIDEWALK DETAILS

PREIN & NEWHOF
CONSULTING ENGINEERS

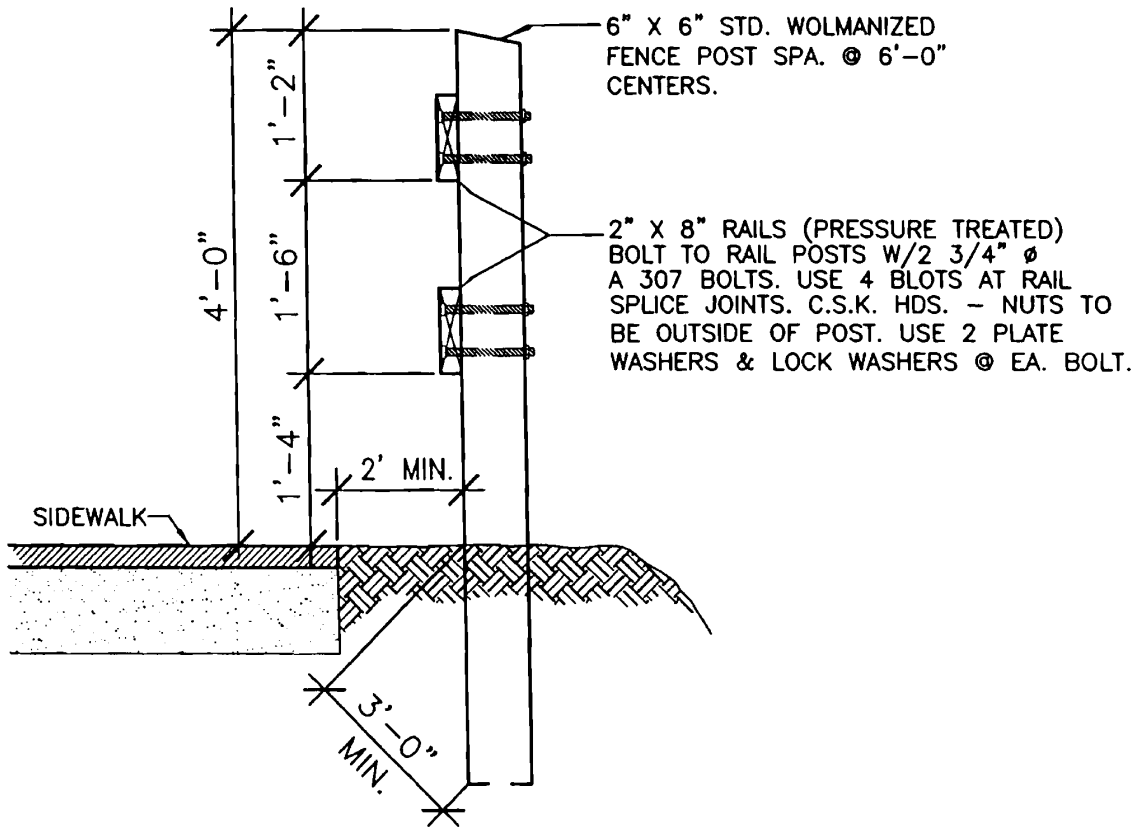




(6" x 6" TREATED LUMBER, 0.4 RETENTION,
EQUIVALENT TO WOOD FOUNDATION
SOUTHERN YELLOW OR RED PINE - SMOOTH SAWN)

TYPICAL TREATED LUMBER RETAINING WALL DETAIL

PREIN & NEUHOFF
CONSULTING ENGINEERS



TYPICAL TREATED LUMBER FENCE DETAIL

PREIN & NEWHOF
CONSULTING ENGINEERS



Memo

To: Oshtemo Charter Township Board
From: Marc Elliott, P.E., Director of Public Works
Date: April 6, 2018
Subject: Amtrak Rail Crossing Update

OBJECTIVE

This memo is to update the Board regarding discussions with Amtrak and others in regard to the design-review and planned construction of a pedestrian crossing of the high-speed rail at Drake Road, between KL Avenue and West Michigan Avenue. As the underlying land owner, Amtrak is the last remaining segment of work to be engineered, acquired and owner-approval for Oshtemo's proposed non-motorized facility located along the west side of Drake Road, and extending from Green Meadow Drive to West Michigan Avenue.

Having exhausted all options, the Supervisor is now prepared to execute the \$84,810 contract (as authorized by the Board on March 27) which Amtrak provided. The Contract is for Amtrak to review the Township's proposed crossing. In addition, we now understand that Amtrak's schedule is approximately 6-months. This will cause the earliest possible construction date for this segment to be the 2019 season.

BACKGROUND

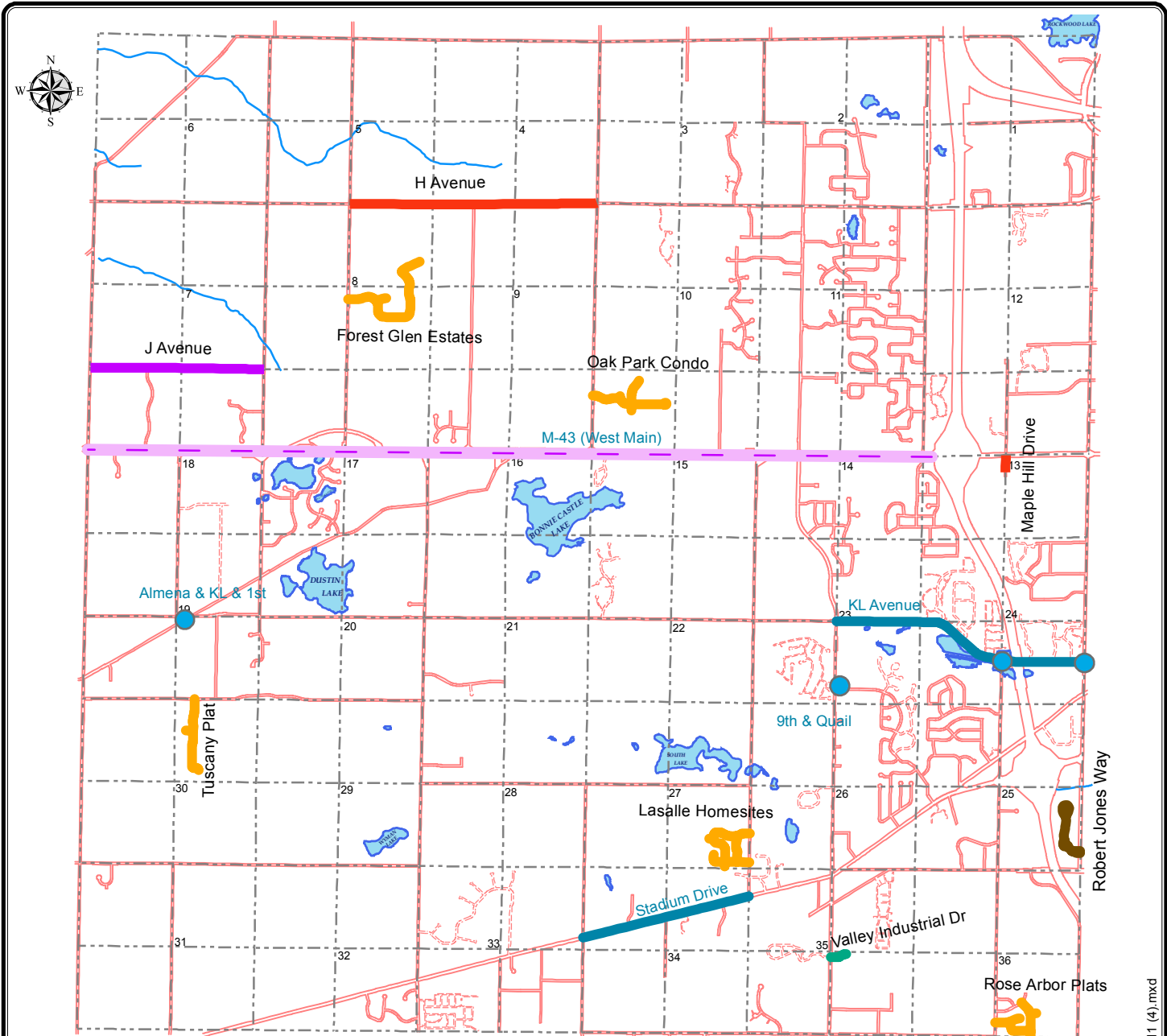
On March 27, 2018 the Township Board did authorize Supervisor Heiny-Cogswell to execute a \$84,810 contract with Amtrak for Amtrak to review the design plans prepared to construct a rail crossing for the Township's proposed Drake Road non-motorized facility. The Board's approval was motivated by the necessity of timeliness to allow 2018 construction. The Board also conditioned Supervisors execution of the Contract to be contingent upon further Township pursuit of viable options to possibly reduce the expense.

A conference call initiated by Oshtemo Township was held on March 29th with multiple MDOT program managers, local Act 51 agency representatives and our Consulting Project Design Team. In summary, the MDOT representatives were unable to accommodate or perceive a viable, alternative path forward for the three project segments proposed for the overall facility.

The overall Drake Road project is a non-motorized facility extending from West Main to Stadium Drive. Over the past four years the project became segmented into three construction projects with two Act 51 Agents (City of Kalamazoo and RCKC). Each segment is associated with an MDOT funding amount. The conference call confirmed that the rail crossing was required in order to receive MDOT financing for two of the segments. MDOT's representatives, having jokingly noted Amtrak as a "nation of its own" remarked upon the need to acquiesce to Amtrak. Please recall that Oshtemo also sought assistance and advice from Representative Upton's Office, as well soliciting the offices of Senators Peters and Stabenow.

INFORMATION PROVIDED

The Amtrak Contract with initial briefing materials is available on-line, in the Board's Agenda Packet for February 13, 2018



Local Road Pavement Treatment	
	Crack Fill and Chip Seal
	Crack Fill, Chip Seal and Fog Seal
	HMA wedging, Crack Fill & Chip Seal
	HMA Overlay
	OSH - New Local Road
	PRIMARY ROAD PROJECTS (RCKC)
	STATE HIGHWAY PROJECTS (MDOT)

2018 Oshtemo Roadway Projects

PROJECTS	TREATMENT	LENGTH	COST
HIGHWAY & PRIMARY ROADS			
M-43 (West Main Street) - MDOT	Crack Fill, HMA Micro-Overlay	26400	1,028,000
9th St at Quail Run	Traffic Signal Upgrades	na	234,000
Almena Dr at Kl Ave & at 1st St	Intersection Realignments & Closure	na	625,000
Kl Ave at 11th St & at Drake Rd	Traffic Signal Upgrades	na	75,000
Kl Ave-9th St to Drake Rd	HMA Overlay & Lane addition E of 11th	10560	1,035,000
Stadium Dr-6th to 8th	Mill and HMA Overlay	5280	450,000
LOCAL STREETS			
H Ave-3rd St to 6th St	HMA Overlay	7879	222,181
J Ave-Vankal St to 2nd St	HMA wedging, Crack Fill & Chip Seal	5538	52,503
Maple Hill Dr-510' S of M-43 to M-43	HMA Overlay	510	21,694
Valley Industrial Dr-9th St to 610' E of 9th St	Crack Fill and Chip Seal	610	12,961
RESIDENTIAL NEIGHBORHOODS			
Forest Glen Estates	Crack Fill, Chip Seal and Fog Seal	4863	51,423
Lasalle Homesites	Crack Fill, Chip Seal and Fog Seal	4458	67,438
Oak Park	Crack Fill, Chip Seal and Fog Seal	3333	38,871
Rose Arbor	Crack Fill, Chip Seal and Fog Seal	3110	41,272
Tuscany Plat	Crack Fill, Chip Seal and Fog Seal	2608	29,401
NEW - Robert Jones Way	New (BTR 2.0)	2926	384,000
All Projects Sum:		14.2 miles	\$4,368,744
State Highway Sum:		5.0 miles	\$1,028,000
Primary Road Sum:		3.0 miles	\$2,419,000
Local Road Sum:		6.2 miles	\$921,744
RCKC Participation in Local Projects = \$265,301			
Oshtemo Local Road Projects = \$656,433			

Date: 4/11/2018



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Department of Public Services

Stockbridge Facility

415 Stockbridge Avenue
Kalamazoo, MI 49001-2898

Ph.269.337.8660

Fx.269.337.8533

Media Release

FOR IMMEDIATE RELEASE

Contacts: James J. Baker, PE
Public Services Director
269-337-8148

2018 SPRING HYDRANT FLUSHING

April 11, 2018, Kalamazoo MI - The City of Kalamazoo Public Services Department announces that the Spring Hydrant Flushing Program for 2018 will begin on Sunday, April 15, 2018 and continue through Tuesday, April 24, 2018. Most of the flushing will be done at night between the hours of 10:00 p.m. and 6:00 a.m. with some additional day flushing between the hours of 7:00 a.m. and 3:30 p.m.

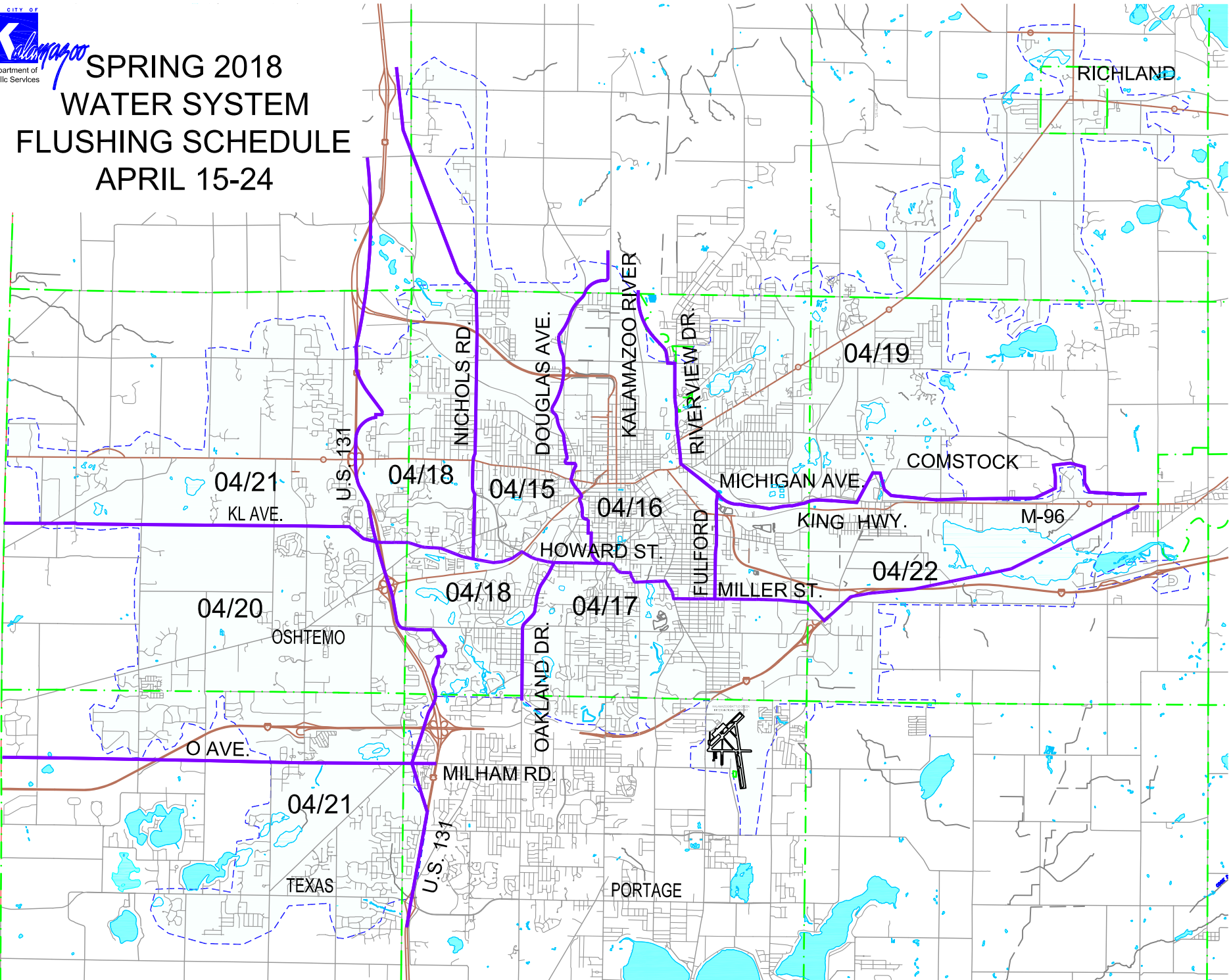
Public Services Department personnel will flush the hydrants and mains to remove accumulated sediments. While flushing doesn't affect the safety of the water supply, tap water may appear cloudy or discolored immediately after flushing. The City is advising residents to run their cold water in their bathtub or utility sink until it runs clear again, and is discouraging them from using their washing machines the day of flushing and for 24 hours after, since clothing could become discolored.

Periodic flushing is necessary to get rid of deposits that form inside the water mains. Over time, sediment builds up on the inside of water distribution pipes, narrowing the path that water can flow through to individual homes and businesses. Flushing stirs up water in the mains and forces water and sediments out. By widening the path that water can flow through the mains, the flushing program will improve water flow, helping the City to better meet the needs of Public Safety fire personnel. Another goal of the flushing program is to make sure that all water supply systems work, including pumps, storage tanks, mains, valves, and hydrants.

If you have scheduled construction or maintenance projects during this time, please contact Robert McClenney at 337-8148 so that necessary precautions can be taken to avoid conflicts or problems.

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SPRING 2018 WATER SYSTEM FLUSHING SCHEDULE APRIL 15-24





FOR IMMEDIATE RELEASE

Contact: *Go!* Green Oshtemo Team
Supervisor Libby Heiny-Cogswell
Clerk Dusty Farmer
Parks Director Karen High
Planning Director Julie Johnston
Public Works Specialist Jamie Baker
Email: oshtemo@oshtemo.org
Office phone: 269-375-4260

***Go!* Green Oshtemo: Community Input Meeting**

KALAMAZOO, MICH., Feb. 20th, 2018 – On Monday, March 5, 2018, Oshtemo Township is inviting community members to help kick off ***Go!* Green Oshtemo**, a comprehensive planning project for parks, recreation, greenways, non-motorized transportation, and cemeteries. The evening meeting will be an exciting collaboration between the Township’s project team and community members in order to get input on all things green. Oshtemo wants to hear from YOU! All ages are encouraged to attend the meeting hosted at the Township Hall, 7275 West Main Street, from 7:00-8:30 p.m. Snacks will be provided.

As Oshtemo Township’s population continues to grow, people are asking for a quality of life that is more connected by sidewalks and paths, more thoughtful of natural features and open spaces, and provides park amenities for recreation activities. The Township is accepting this challenge, rolling up our collective sleeves, and inviting the community to work with us on these requests. Recent accomplishments such as the completion of the Rural Preservation Strategy within the 2018 Master Plan will be shared, while new ideas are encouraged.

Township Supervisor Libby Heiny-Cogswell states that, “Oshtemo is changing and it is important for the community to step up to create a strategy for conservation and amenities.”

Please plan to attend this important meeting. As always, kids are invited to join the conversation to share their recreation wants and needs!

For more information about this community input meeting, please contact a staff member of the ***Go!* Green Oshtemo** team at 269-375-4260.

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Event Location: Oshtemo Township Town Hall at 7275 W. Main Street Kalamazoo, MI 49009