

**OSHTEMO CHARTER TOWNSHIP BOARD**  
7275 West Main Street, Kalamazoo, MI 49009  
269.375.4260

**August 28, 2018**

**BUDGET WORKSHOP**

**5:00 p.m.**

**AGENDA**

- A. Call to Order
- B. Public Comment
- C. Discussion on 2019 Budget (Continued)

**REGULAR MEETING**

**7:15 p.m.**

**AGENDA**

- 1. Call to Order
- 2. Pledge of Allegiance
- 3. Public Comment on Non-Agenda Items
- 4. Consent Agenda
  - a. Approve Minutes – August 14, 2018 Regular Meeting
  - b. Receipts & Disbursements Report
  - c. Public Works Sewer & Water Funds Budget Amendments
- 5. 2019 HAZMAT Mutual Aid Agreement
- 6. Consideration of Sidewalk/Shared-Use Path Construction Standards Amendment
- 7. Consideration of Complete Streets Policy Amendments
- 8. Consideration of Sidewalk Projects (in Conjunction with Sanitary Sewer Expansion)
- 9. Township Noticing Requirements
  - a. Discussion on Publication Methods
  - b. Consideration of Noticing Budget Amendment
- 10. Other Township Business
- 11. Public Comment
- 12. Board Member Comments
- 13. Adjournment

**Policy for Public Comment**  
**Township Board Regular Meetings, Planning Commission & ZBA Meetings**

All public comment shall be received during one of the following portions of the Agenda of an open meeting:

- a. Citizen Comment on Non-Agenda Items or Public Comment – while this is not intended to be a forum for dialogue and/or debate, if a citizen inquiry can be answered succinctly and briefly, it will be addressed or it may be delegated to the appropriate Township Official to respond at a later date.
- b. After an agenda item is presented by staff and/or an applicant, public comment will be invited. At the close of public comment there will be board discussion prior to call for a motion.

Anyone wishing to make a comment will be asked to come to the podium to facilitate the audio/visual capabilities of the meeting room. Speakers will be invited to provide their name; it is not required unless the speaker wishes to have their comment recorded in the minutes.

All public comment offered during public hearings shall be directed, and relevant, to the item of business on which the public hearing is being conducted. Comment during the Public Comment or Citizen Comment on Non-Agenda Items may be directed to any issue.

All public comment shall be limited to four (4) minutes in duration unless special permission has been granted in advance by the Supervisor or Chairperson of the meeting.

Public comment shall not be repetitive, slanderous, abusive, threatening, boisterous, or contrary to the orderly conduct of business. The Supervisor or Chairperson of the meeting shall terminate any public comment which is in contravention of any of the principles and procedures set forth herein.

(adopted 5/9/2000)  
(revised 5/14/2013)

**Policy for Public Comment**  
**6:00 p.m. "Public Comment"/Portion of Township Board Meetings**

At the commencement of the meeting, the Supervisor shall poll the members of the public who are present to determine how many persons wish to make comments. The Supervisor shall allocate maximum comment time among persons so identified based upon the total number of persons indicating their wish to make public comments, but no longer than ten (10) minutes per person. Special permission to extend the maximum comment time may be granted in advance by the Supervisor based upon the topic of discussion.

While this is not intended to be a forum for dialogue and/or debate, if a citizen inquiry can be answered succinctly and briefly, it will be addressed or it may be delegated to the appropriate Township Official to respond at a later date.

Anyone wishing to make a comment will be asked to come to the podium to facilitate the audio/visual capabilities of the meeting room. Speakers will be invited to provide their name; it is not required unless the speaker wishes to have their comment recorded in the minutes.

Public comment shall not be repetitive, slanderous, abusive, threatening, boisterous, or contrary to the orderly conduct of business. The Supervisor shall terminate any public comment which is in contravention of any of the principles and procedures set forth herein.

(adopted 2/27/2001)  
(revised 5/14/2013)

# BUDGET AMENDMENT REQUEST

(Requesting funds for a line item in addition to the approved budget)

Date: 08/14/2018

Department Head Name: M Elliott/Public Works

Fund Name: 491 Water

		Amount	
Additional Funds Request for:	491-000-96600	Curb Box Installation	<span style="border: 1px solid black; padding: 2px;">\$ 25,000.00</span>
(description and GL number)			
			<span style="border: 1px solid black; padding: 2px;">\$ 25,000.00</span>
Funds requested from:	491-000-40100	Carryover	<span style="border: 1px solid black; padding: 2px;">\$ 25,000.00</span>
(description and GL number)			
			<span style="border: 1px solid black; padding: 2px;">\$ 25,000.00</span>

Explanation of request:

A pickup in new developments has created additional service demands for this work. Additionally, costs for additional length of pipe have exceeded the cost-averaged projections. Also noteworthy is that revenue (water connection fees) projected to be collected in 2018 fees were lowered to reflect a lower contract-cost for the work. However, about half the current expenditures has gone toward work committed in 2017 at the higher construction cost. That is, although higher fees had been collected in 2017 for this work, the FY 2018 carryover budget did not fully pull into 2018 the higher fees that had been collected in 2017.

Supervisor Review: LHC 8-24-2018

(pending or date reviewed)

Board Authorization:

(pending or date authorized)

# BUDGET AMENDMENT REQUEST

(Requesting funds for a line item in addition to the approved budget)

Date: 08/14/2018

Department Head Name: M Elliott/Public Works

Fund Name:

		Amount	
Additional Funds Request for: (description and GL number)	<u>490-000-96400.WWKLA</u>	<u>Construction Costs (KL Ave Sanitary)</u>	<u>\$ 7,000.00</u>
	_____	_____	_____
	_____	_____	<u>\$ 7,000.00</u>
Funds requested from: (description and GL number)	<u>490-000-40100</u>	<u>Carryover</u>	<u>\$ 7,000.00</u>
	_____	_____	_____
	_____	_____	_____
	_____	_____	_____
	_____	_____	<u>\$ 7,000.00</u>

Explanation of request:

Oshtemo's engineering consultant is continuing to work with the KL Avenue Sewer Project Contractor to resolve restoration issues as well as addressing a discovered roadway drainage concern. These 2018 expenditures were not anticipated in the Board's original 2018 budget. These amended-budget additional expenditures will be split among the Contractor (for additional services to resolve road drainage) and the consultant (for oversight and coordination). [Actual fees billed by consultant to-date have been \$14,000 less than the project proposal.]

Supervisor Review:

(pending or date reviewed)

Board Authorization:

(pending or date authorized)



# Memo



To: Libby Heiny-Cogswell, Supervisor and Oshtemo Township Board  
From: Mark Barnes, Fire Chief  
Date: August 17, 2018  
Re: Updated Countywide Hazardous Materials Response Agreement

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In 2010 municipalities in Kalamazoo County signed an “Intergovernmental Fire Protection, Hazardous Materials Incident Response Agreement” which gave official birth to the current countywide Hazardous Materials (Haz-Mat) team. This team is made up of members from various fire agencies in Kalamazoo County with the assistance of some local private sector companies.

The team’s primary mission is to respond emergently to incidents involving the release of hazardous materials where they will identify the product and then initiate coordinated efforts to stop its spread or contain the hazard. In some cases, the product is an unknown substance which makes identifying it the most important task.

They are not responsible for cleanup but will supervise that it is accomplished in a safe and complete manner. They also act on our behalf to coordinate efforts of various agencies such as Michigan’s Department of Natural Resources (DNR) and Department of Environmental Quality (DEQ). In the case of a transportation related event (spill on a roadway, rail or other), they will also provide Haz-Mat guidance to involved agencies such as the Department of Transportation (M-DOT).

Finally, they assist with invoice coordination as the local unit of government bills the costs back to the identified spiller. This agreement allows contiguous municipalities to Kalamazoo County to also participate and thus receive this service. Note: Only the metro communities do not have a boarder with an adjoining county. All other Townships are impacted by events in neighboring county jurisdictions including Oshtemo.

As in the last agreement, the annual membership fee remains at \$1,400 per agency. This helps to pay for maintenance of the equipment and vehicle. Each participating agency covers the cost of having their members on the team such as time for monthly training and periodic responses.

This is an astounding example of a highly beneficial intergovernmental cooperative effort. Although any participating unit of government may withdraw with a 30-day notice, the agreement is designed to remain in effect for three years (1/1/2019 – 12/31/2021).

**Recommendation:** That Oshtemo Township continues its membership with the Kalamazoo County Hazardous Materials Response Team (KCHMRT) and sign the updated agreement.



**INTERGOVERNMENTAL FIRE PROTECTION  
HAZARDOUS MATERIALS INCIDENT RESPONSE AGREEMENT**

This Intergovernmental Fire Protection Hazardous Materials Incident Response Agreement, dated the 31 day of December, 2018, between the undersigned municipalities, is as follows:

WHEREAS, the specialized services of a hazardous materials response team are above and beyond the normal response services provided on a reciprocal basis under the provisions of the Mutual Aid Fire Agreement entered into by the parties; and

WHEREAS, there is a need to facilitate and coordinate the recovery of the cost of response activity to hazardous materials incidents as permitted by Section 20126a (M.C.L. 324.20126a) of the Michigan Natural Resources and Environmental Protection Act, (NREPA) Public Act 451 of 1994; and,

WHEREAS, each party desires to enhance and improve responses to hazardous materials incidents occurring within Kalamazoo County, and,

WHEREAS, 1951 P.A. 35, as amended, being MCL 124.1 *et seq.*, provides that municipalities may enter into cooperative agreements to provide services to and for each other; and,

WHEREAS, this Hazardous Materials Incident Response Agreement is intended to assist the undersigned municipalities in providing timely and efficient responses to hazardous materials incidents within Kalamazoo County.

NOW, THEREFORE, in consideration of the mutual undertakings of the parties hereto, IT IS AGREED:

1. Definitions

- A. **Financial Custodian** means the custodian of response activity costs and annual dues as provided in paragraph 4 of this agreement.
- B. **Fire Chief** includes the fire chief, the fire chief's designated representative or such member of the fire department as is in charge of the fire department emergency and technical response personnel, or the chief of a public safety department or that official's designated representative.
- C. **Incident Management System:** The National Incident Management System (NIMS) required for managing a hazardous materials incident by state and federal regulation.
- D. **Member in Good Standing** means a municipality that has entered into this agreement and is current in the payment of dues and compliant with the obligations specified in this agreement.

- E. **KCHMT member** means an employee of a participating municipality who becomes a member of the Kalamazoo County Hazardous Materials Response Team, (KCHMT) as established pursuant to this agreement.
- F. **Participating Municipality** means a municipality that has entered into this agreement and is a member in good standing.
- G. **Recovery Costs** mean the costs associated with recovery of response activity costs as defined by this Agreement, including, but not limited to attorney fees, court filing fees, witness fees and other costs of litigation and collection.
- H. **Requesting Department:** The fire department serving a participating municipality in which the hazardous materials incident exists and that requests aid under this Agreement.
- I. **Responding Department:** Any and all fire departments serving a participating municipality that provide personnel (including personnel who respond as a member of the Kalamazoo County Hazardous Materials Response Team) or equipment under this Agreement. The Kalamazoo County Office of Emergency Management and responding EMS providers are included in this definition.
- J. **Response Activity** means evaluation, interim response activity, remedial action, demolition, or the taking of other actions necessary to protect the public health, safety, or welfare, or the environment or the natural resources consistent with the rules relating to the selection and implementation of response activity promulgated under the Michigan Natural Resources and Environmental Protection Act.
- K. **Response Activity Costs** or **Costs of Response Activity** means all costs incurred by the requesting department and all responding departments, including the KCHMT, in taking or conducting a response activity, including all costs, that a municipality may recover pursuant to Section 20126a of the Michigan Natural Resources and Environmental Protection Act, MCL 324.20126a. These costs may include, but not be limited to, administrative fees assessed by the financial custodian to services provided relative to the collection of response activity costs, wages and/or salaries except for those employees who are already on duty and being paid; costs of vehicles responding at either the MEMAC reimbursement rate or another verified manner of calculating operating costs; costs of replacing equipment and supplies that are expended and not reusable; costs of decontaminating or cleaning equipment or other costs necessary to put that equipment back in service; costs of personnel support including, but not necessarily limited to, food, lodging, portable rest room facilities, or transportation; costs to rent, purchase or otherwise obtain equipment, supplies, or other material

necessary to mitigate the incident; costs to contract with specialized response personnel or organizations.

- L. **MEMAC Reimbursement Rate** means the reimbursement rates for apparatus and other equipment used to mitigate incidents contained in the Michigan Emergency Management Assistance Compact.

2. Kalamazoo County Hazardous Materials Response Team

- A. Participating municipalities agree to form and jointly operate the Kalamazoo County Hazardous Materials Response Team, (KCHMT) the purpose of which is to assist local fire departments by providing a trained, equipped team of on-scene support to the incident commander of the requesting department for the provision of response activity to a hazardous materials incident.
- B. The KCHMT shall be managed by an executive board as provided for in the by-laws.
- C. The executive board shall set all operating standards, policies and procedures and shall establish criteria for individual membership on the team.
- D. The executive board shall elect one member to serve as chair of the board and other such officers as may be required. Elected officers will serve a one-year term of office, corresponding to the fiscal year, and may serve consecutive terms.
- E. The KCHMT shall maintain By-laws, an Operations Manual and a listing of equipment owned by the team. An updated copy of each, along with a report of annual KCHMT activity shall be provided to any requesting municipality annually, by March 1.
- F. Response Procedures
  - 1. A Requesting Department may request the services of the KCHMT following dispatch and response procedures as outlined in the KCHMT Operations Manual.
  - 2. The KCHMT, when responding to an incident, shall operate within the Incident Management System put in place by the Requesting Department. Command of the incident shall remain with the Requesting Department.
  - 3. If a Responding Department's personnel arrive prior to the Requesting Department's units or personnel, the assisting personnel will establish "Incident Command" as prescribed in NIMS and proceed with emergency operations. Upon arrival of a command officer from the Requesting Department, "Incident Command" will be passed to the Requesting Department.



4. Assisting personnel and units will be released and returned to their jurisdiction as soon as practical.

3. Membership Dues

Each municipal fire department serving a participating municipality or municipalities will be required to pay annual dues in the amount of \$1,400.00 per fiscal year.

- A. The municipal fire departments will be invoiced not later than October 1 for dues for the next calendar year. The department will be in arrears if the dues are not received by January 1.

4. Custodian of Dues and Recovered Costs

- A. The County of Kalamazoo will be the custodian of all dues and costs recovered pursuant to paragraph 9 of this agreement. The dues and recovered costs shall be placed in separate accounts and maintained in accordance with standard municipal accounting procedures.

5. Geographical Area of Coverage

The KCHMT shall provide assistance to any Requesting Department within Kalamazoo County. The KCHMT may also provide assistance to municipalities adjoining Kalamazoo County in which there is a written mutual aid agreement. All requests for KCHMT assistance outside of Kalamazoo County must be made by a requesting department that is on scene of the incident, or the Battle Creek Hazmat Team.

6. Personnel and Equipment Standards

KCHMT members and equipment shall meet the following minimal levels of training or recognized industry standards:

- A. All KCHMT members shall be certified to the Hazardous Materials Technician level by a representative of the municipality employing the member. However, the executive board of the KCHMT may approve team membership for individuals certified to the Hazardous Materials Operations level if such membership has been determined to provide a benefit to the team.
- B. Equipment used shall comply with all applicable rules and regulations including, but not limited to, CFR 1910.120, MIOSHA Part 432, and standards adopted by the executive board.

7. Training and Medical Examinations

- A. KCHMT members shall meet all statutory and training requirements applicable to the team position being filled.
  - 1. The KCHMT will conduct regular training as approved by the Executive Board to allow KCHMT members to remain competent in necessary skills. KCHMT members are expected to comply with training requirements.
  - 2. KCHMT members who fail to maintain training requirements or required certifications, as established by the executive board of the KCHMT, may be removed from KCHMT membership by action of the executive board. Such action is not subject to appeal by the participating municipalities.
  - 3. Employees of the participating municipalities who are KCHMT members shall attend monthly training sessions of the team as part of their employment by the participating municipality that employs them.
- B. It is the responsibility of the participating member that is an employer of a KCHMT member to provide periodic physical examinations that meet the requirements of 29 CFR 1910.120. The frequency of these examinations may range from twelve to twenty-four months based on the recommendation of the physician. Prior to joining the KCHMT, KCHMT member applicants shall undergo a physical examination to identify any pre-existing medical conditions and to record baseline measurements that may change over time.

8. Liability and Mutual Release

- A. Each participating municipality providing KCHMT members to KCHMT shall be solely responsible for all costs and expenses associated with its employees who are KCHMT members and equipment utilized by its employees who are KCHMT members in responding to emergencies or participating in training under this Agreement, including without limitations, wages and fringe benefits, workers' compensation, disability or pension contributions, and the purchase, repair or maintenance of equipment and fire apparatus.
- B. Each participating municipality shall only be responsible for claims, demands, costs (including attorney fees) and damages, for death, bodily injury, or property damage arising directly from the acts or omissions of its individual personnel.
- C. Each participating municipality shall defend, indemnify and hold all other participating municipalities including the elected officials, agents and employees (hereinafter collectively as "Indemnitee") of the other participating municipalities harmless from all claims, demands, costs (including attorney's fees) and damages for death, bodily injury, or property damage arising directly from the acts or omissions of its individual personnel, employees or agents. Provided, however, that if such death, injury or damage is caused by personnel, employees or agents of multiple participating municipalities, each participating municipality shall then be responsible for its percentage of responsibility, and shall indemnify the other participating municipalities so that each pays in proportion to its responsibility.

- D. In providing the indemnification set forth above, the Indemnitor is not waiving any defense as is otherwise available to it by law, provided such defenses are also available and asserted by the Indemnitor for the benefit of the Indemnitee. The Indemnitor shall not be responsible for the indemnification obligation set forth above with respect to the Indemnitee to the extent that the Indemnitee has waived a defense that was otherwise available to it by law. The Indemnitor shall have the option to settle any claim, demand or liability on such terms as it shall determine.
- E. No fire fighter or emergency medical personnel from any Department shall be considered a drafted person of another Department when performing services in furtherance of this Agreement under MCL 123.401 through MCL 123.403.

9. Recovery or Reimbursement of Response Activity Costs

- A. As soon as practicable after providing response activity to a particular hazardous materials incident:
  - 1. The KCHMT executive board will submit an itemized statement of all response activity costs (excluding labor costs) incurred by the KCHMT in response to the particular incident to the Requesting Department, and provide a copy of such statement to each Responding Department and the County of Kalamazoo.
  - 2. Each Responding Department will submit an itemized statement of all response activity costs (including labor costs) incurred by the Responding Department in response to the particular incident to the Requesting Department, and provide a copy to all other Responding Departments, the KCHMT, and the County of Kalamazoo.
  - 3. The Requesting Department will submit an itemized statement of all recovery costs incurred by the Requesting Department pursuant to paragraph 9(C) below to all Responding Departments and the KCHMT prior to distribution of recovered costs pursuant to paragraph 9 (D) below.
- B. The Requesting Department will pay the response activity costs of the KCHMT with respect to materials owned and used by the KCHMT in connection with providing response activity to a particular hazardous materials incident within 30 days of receipt of the itemized statement from KCHMT or on a payment schedule agreed to by the Executive Board. The Requesting Department will be responsible for the payment of these costs irrespective of any recovery of response activity costs as provided in paragraph 9(C) below. The KCHMT will provide an accounting of all response activity costs paid to it by the Requesting Department pursuant to this paragraph to all Responding Departments and the County of Kalamazoo prior to distribution of recovered costs pursuant to paragraph 9 (D) below.

- C. Recovery of all response activity costs incurred by the Requesting Department, the KCHMT, and any Responding Department as provided by the Michigan Natural Resources and Environmental Protection Act (NREPA) will be the responsibility of the Requesting Department. The Requesting Department will be responsible for all recovery\_costs associated with recovery of the response activity costs. The obligation of the Requesting Department to recover response activity costs may be waived by a vote of at least two thirds of the executive board of the KCHMT upon a determination that the expense of cost recovery would not be cost effective. In that event, each participating municipality may elect to recover its own costs.
- D. All costs recovered by the Requesting Department as provided in paragraph C, will be deposited with the County of Kalamazoo as the custodian of such funds. Response activity costs will then be paid first to the KCHMT in accordance with its itemized statement, to the extent response activity costs of KCHMT have not been fully reimbursed by the Requesting Department as shown on the accounting for payment of same pursuant to paragraph 9 (B), above. The response activity costs of the Requesting Department and each Responding Departmental will then be paid from the remaining funds recovered. If the remaining funds recovered will not fully reimburse the Requesting Department and each Responding Department, then the remaining funds will be distributed to the Requesting Department and each Responding Department proportionate to the itemized statement prepared by each Department and each Department will be solely responsible for its unreimbursed costs.

10. Multiple Demands For Service

If any participating municipality is confronted with a simultaneous demand to assist a fire department of another governmental unit or respond to a fire/rescue call within that municipality's jurisdictional boundaries, the command officer in charge of that municipality's responding fire/rescue equipment and personnel shall have the discretion to deploy that equipment and personnel for their best utilization in the interest of public health, safety and welfare and with the recognition of that municipality's primary obligation to respond within its own jurisdiction.

11. Effect Upon Other Agreements

The participating municipalities have also entered into a Mutual Fire Protection Contract, dated March 6, 1987. That contract shall remain in effect and shall not be changed by this agreement.

12. Effective Date, Term, Dissolution, and Termination of Membership

This Agreement is effective as of December 31 2018, and shall continue in force until January 1, 2022, or dissolved by a majority vote of the governing bodies of the participating members. A participating member may terminate its membership with or without cause, upon thirty (30) days written notice to the other participating members. Such termination of membership shall not effectuate the dissolution of this agreement or the KCHMT.

IN WITNESS THEREOF, those parties hereto have executed this Agreement on the date indicated above.

CITY OF PORTAGE

By \_\_\_\_\_

Its \_\_\_\_\_

CITY OF KALAMAZOO

By \_\_\_\_\_

Its \_\_\_\_\_

CHARTER TOWNSHIP OF KALAMAZOO

By \_\_\_\_\_

Its \_\_\_\_\_

CHARTER TOWNSHIP OF OSHTEMO

By \_\_\_\_\_

Its \_\_\_\_\_

CHARTER TOWNSHIP OF COMSTOCK

By \_\_\_\_\_

Its \_\_\_\_\_

CHARTER TOWNSHIP OF TEXAS

By \_\_\_\_\_

Its \_\_\_\_\_

SOUTH KALAMAZOO COUNTY FIRE  
AUTHORITY

By \_\_\_\_\_

Its \_\_\_\_\_

CHARTER TOWNSHIP OF COOPER

By \_\_\_\_\_

Its \_\_\_\_\_

CHARLESTON TOWNSHIP

By \_\_\_\_\_

Its \_\_\_\_\_

CITY OF GALESBURG

By \_\_\_\_\_

Its \_\_\_\_\_

PAVILION TOWNSHIP

By \_\_\_\_\_

Its \_\_\_\_\_

ROSS/AUGUSTA FIRE DEPARTMENT

By \_\_\_\_\_

Its \_\_\_\_\_

CLIMAX TOWNSHIP

By \_\_\_\_\_

Its \_\_\_\_\_

CITY OF PARCHMENT

By \_\_\_\_\_

Its \_\_\_\_\_

RICHLAND TOWNSHIP

By \_\_\_\_\_

Its \_\_\_\_\_

ALAMO TOWNSHIP

By \_\_\_\_\_

Its \_\_\_\_\_

COUNTY OF KALAMAZOO

By \_\_\_\_\_

Its \_\_\_\_\_

VILLAGE OF CLIMAX

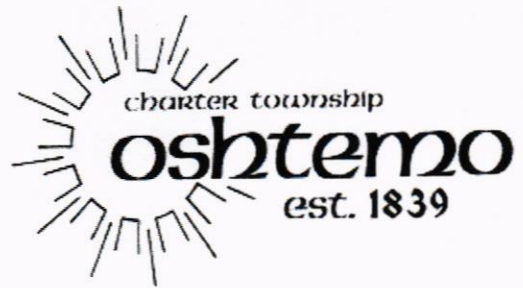
By \_\_\_\_\_

Its \_\_\_\_\_

VILLAGE OF RICHLAND

By \_\_\_\_\_

Its \_\_\_\_\_



# Memo

**To:** Oshtemo Charter Township Board  
**From:** Marc Elliott, P.E., Director of Public Works  
**Date:** August 23, 2018  
**Subject:** Amendment to Oshtemo Construction Standards, Sidewalks and Shared-Use Paths

## **OBJECTIVE**

An amendment to Oshtemo Township's Sidewalk/Shared-Use Path Construction Standards is proposed to assure a consistent interpretation and expectations of the work extent when public sidewalks are an item of work for new construction, as a retrofit within current development or as a publicly sponsored non-motorized facility enhancement. This amendment clarifies when accessible pathways in-pavement are acceptable for some commercial driveway entrances.

## **BACKGROUND**

The amended Standard was unanimously endorsed by members of the Oshtemo Township Capital Improvement Committee (CIC) at their July 2018 meeting. The CIC had been consulted by staff seeking policy guidance and clarification regarding methods of accessible pathway construction for site conditions likely to be encountered along the route. The amendment establishes an objective basis for determining if the walkway should be constructed through commercial pavements as a dissimilar structure of dissimilar material (e.g. a narrow, concrete slab). In the case of commercial asphalt entrance drives, the repeated cycling and transfer of heavy vehicle-loads between these detached and dissimilar pavements can be detrimental to the life expectancy of both. Freeze-thaw cycles and mechanical snow plowing perpendicular to the abutting faces of the joint are also problematic. The proposed amendment will allow an accessible pathway over continuous driveway pavements where appropriate, and require concrete aprons with abutting concrete walks for new construction where concrete curb and gutter is present.

## **INFORMATION PROVIDED**

A redline markup with the single-paragraph amendment is attached.

## **STATEMENT OF WHAT YOU ARE ASKING BOARD TO APPROVE**

Board action in accordance with Ordinance No. 468 (294.005 Sec V. B) to approve, as amended, the 2018 Oshtemo Township Sidewalk/Shared-Use Path Construction Standards.





## OSHTEMO TOWNSHIP SIDEWALK/SHARED-USE PATH CONSTRUCTION STANDARDS

*Adopted August 9, 2016/Draft July 18, 2018*

### CONCRETE SIDEWALK CONSTRUCTION

The construction of Sidewalks and Shared-Use Paths within Oshtemo is managed through the Township's issuance of a Sidewalk/Non-Motorized Path Permit. The permitting process includes both a pre-pour inspection of the base and concrete forms, and a final project inspection for acceptance of the work. Concrete sidewalk shall conform to MDOT 2012 (or current edition) Standard Specifications for Construction Section 803, "Concrete Sidewalks, Sidewalk Ramps and Steps" and shall be a minimum of five (5) feet wide unless a different width is required by other Township ordinances or regulations.

#### Driveway Sidewalk Crossings

Where public sidewalks (AKA pedestrian route) cross residential driveways, the sidewalk shall be constructed of concrete through the driveway. Where a curb-line concrete gutter pan begins the driveway, the driveway apron between the curb and sidewalk shall also be constructed of concrete. Hot-Mix Asphalt (HMA) commercial driveways that lack a concrete roadway gutter, and which have greater than two lanes or heavy traffic may seek administrative approval to establish a pedestrian route over the driveway in lieu of placing a concrete walkway through the HMA material. When new sidewalks are extended through existing driveways, it shall be administratively determined by the Township to what extent the existing driveway pavements will need to be reconstructed in lieu of providing a pedestrian route over the pavement.

#### Grade

The sidewalk shall be constructed to match the existing grade, or as noted on the construction drawings. The sidewalk will have a transverse slope either toward or away from the road to maintain existing drainage patterns. Minor fills and cuts will be made in the field during construction to provide smooth transition of the sidewalk and maintain existing drainage patterns.

#### Sub-base Preparation

Existing vegetation shall be removed and topsoil excavated to provide a four (4") inch sand sub-base for the proposed sidewalk. The existing sand sub-base shall be compacted to ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures. Where fill sand is required it shall be compacted to achieve ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures.

### Concrete

Concrete shall meet the requirements for Grade P1 Concrete or Grade S2 Concrete as specified in the MDOT Standard Specifications for Construction Section 601, "Portland Cement Concrete Pavements". Concrete shall be six sack limestone mix and shall be air-entrained and shall have a compressive strength of not less than 3,500 pounds per square inch, within twenty-eight (28) days of paving. Other materials shall meet the requirements of the applicable portions of the MDOT Standard Specifications for Construction. All concrete sidewalks shall be paved with a single course of concrete. Sidewalks through driveways shall be six (6") inches thick. All other walks shall be four (4") inches thick.

### Joints

Full depth transverse expansion joints shall be constructed perpendicular to the surface of the sidewalk at intervals not to exceed fifty (50') feet. Expansion joint material shall be one-half (1/2") inch pre-molded expansion joints and shall be set 1/4" below the surface of the sidewalk. Sealing of joints will not be required. One (1") inch pre-molded expansion joints must be placed between the sidewalk and back-of-curb when sidewalk is constructed between the curb and building or other rigid structures. Sealing of joints will not be required. Transverse plane of weakness joints shall be true to line and grade, and shall be placed at four (4') foot intervals and shall be formed with a grooving tool. Planes of weakness joints shall be constructed to a depth of at least one (1") inch and a width of 1/8 inch to 1/4 inch. Sealing of joints will not be required.

### Surface

The surface of the concrete shall be floated to a level uniform surface and left with a slightly rounded surface. The surface shall be roughened with mechanic's brush to prevent smooth and slippery surfaces. No surface shall be troweled to a glassy finish. Edges at the forms and joints shall be rounded with an edging tool.

### Curb Ramps and Approaches

New roadway construction in locations where non-motorized facilities are anticipated shall incorporate concrete curbing of RCKC standards at intersections. Roadway curbing shall be extended through the roadway intersection, within the right-of-way limits of each approaching roadway. The purpose of this standard is to ensure that all non-motorized ramps, (current and anticipated) are fully integrated into the casting of the concrete curb through the intersection. As required, concrete curbing within intersections shall be extended a sufficient length to assure that the concrete curb carries the transition from the existing edge of pavement design of each approaching roadway. Commonly encountered edge-of-roadway pavement designs include ditch drainage, gravel shoulder, bituminous valley gutter, mountable and high back concrete curb.

Curb ramps shall be concrete unless written approval of an alternative is pre-approved by the Township. In locations where existing concrete curb is without an integrated approach (curb-cut), the entire roadway concrete curb section may need to be removed and re-constructed to meet ADA compliance standards. On a case by case basis, the removal of an existing curb back by saw cutting may be able to yield an ADA compliant transition through the curb. Proposals to use the saw cut method of ramp construction should seek prior approval from the Township.

Ramp that approach existing roads without concrete curbing should be reviewed with the Township prior to ramp construction. If providing new concrete curb for the ramp transition is determined to be impracticable, or if extensive re-construction the intersection curb is determined to be excessive, ADA compliant asphalt transitions onto asphalt paving may be accepted upon prior written approval of the township.

Butt joints must be provided at the transition from concrete to bituminous paving for all non-motorized facilities. Saw cutting, grinding, or similar means of joint formation is required. As practicable, transition butt-joints to asphalt curbing shall be located at the curb high-point. Material transition joints are not allowed along a drainage flow-line. Surface grinding of existing asphalt curbing is an acceptable means to achieve ADA compliant transitions. Alternately, IR asphalt restoration or placement of a new transition segment of asphalt paving can be constructed through the curb profile to achieve the transition from the asphalt roadway to the concrete ramp. Regardless of construction method or prior approval of the means of construction, compliance with ADA standards will need to be demonstrated to receive acceptance of the work.

#### Sidewalk Detectable Warnings

Sidewalk Detectable Warning shall apply to all construction or reconstruction of streets, curbs, or sidewalks. Detectable Warnings shall be placed at all street intersections and at select commercial/industrial driveways where driveway traffic and pedestrian crossings are determined/projected by the Township to generate sufficient conflicts to warrant the greater risk awareness provided by detectable warnings.

Detectable Warnings shall conform to ADA requirements, MDOT Specifications and MDOT Detail R-28-E. Detectable Warnings shall be made of ductile iron material, extend the full width of the curb/sidewalk ramp. They shall be located so that the edge nearest the curb line or street is 6" to 8" from the curb line.

### **HOT MIX ASPHALT SHARED-USE PATH CONSTRUCTION**

Shared-use paths shall be a minimum of ten (10) feet wide unless a different width is required by other Township ordinances or regulations.

#### Grade

The sidewalk shall be constructed to match the existing grade, or as noted on construction drawings. The path will have a transverse slope either toward or away from the road to maintain existing drainage patterns. Minor fills and cuts will be made in the field during construction to provide smooth transition of the path and maintain existing drainage patterns.

#### Sub-base Preparation

Existing vegetation shall be removed and topsoil excavated to provide a six (6) inch sand sub-base for the proposed path. The existing sand sub-base shall be compacted to ninety-five (95%) percent

maximum unit weight in accordance with MDOT procedures. Where fill sand is required it shall be compacted to achieve ninety-five (95%) percent maximum unit weight in accordance with MDOT procedures.

#### Gravel Base

The gravel base shall extend one foot beyond the width of the topcoat on each side and consist of six (6) inches of compacted MDOT 22A gravel in accordance with the section as indicated on the construction drawing. Density of the gravel shall be ninety five (95%) percent.

#### Hot Mix Asphalt (HMA) Surface

The hot mix asphalt surface shall consist of 120 lbs/SY MDOT 36A hot mix asphalt (top) over 165 lbs/SY of MDOT 13A hot mix asphalt (level) placed in accordance with the section as indicated on the construction drawings. The asphalt performance grade shall be 58-28. The hot mix asphalt shall be placed with a self-propelled paver; spreader boxes will not be permitted. A bond coat shall be applied between successive courses of hot mix asphalt at an application rate of 0.1 gal./SY.

#### **REMOVABLE BOLLARDS (Vehicle Exclusion Device)**

Certain Shared-Use Pathways may include removable Bollards as vehicle exclusion devices. When shown on plans, this work shall include the construction of the concrete footings, in-ground receiver, installation of the bollards, and all related work necessary to provide complete Removable Bollards as shown on the plans.

#### Materials

The materials shall be as specified below:

1. Removable Bollard: Model R-7901 Steel Removable Bollard with Removable Mounting and locking hinged steel lid from Reliance Foundry, Petersen Mfg. Co., Inc., or approved equal. Bollard shall be corrosion resistant under coating and safety yellow in color with safety yellow reflective tape at the top.
2. Concrete: Provide grade S1 concrete as specified in Section 701 of the MDOT Standard Specifications for Construction.
3. Steel Reinforcement: Provide steel reinforcement as specified in Section 905 of the MDOT Standard Specifications for Construction.
4. Submittals: The Contractor shall submit complete shop drawings, showing all materials, dimension, fastenings, and fastening devices.

#### Construction

The Removable Bollards shall be constructed according to the plans and as described below.

1. Preparation work shall be thoroughly reviewed with the Engineer prior to installation. Saw cut the existing HMA prior to placement.
2. Install Removable Bollards complete per manufacturer's specifications and instructions and as detailed on approved shop drawings.

3. Concrete: Install concrete components as specified in Section 706 of the MDOT Standard Specifications for Construction and as detailed on the plans.
4. All excess and waste materials shall be disposed of legally off-site. Upon completion of the work, leave areas in a clean condition.

## **SIGNS**

Certain Shared-Use Pathways may include restricted usage or way-finding signs. When shown on plans, this work shall comply with the construction standards of the Kalamazoo County Road Commission and all permit requirements of the applicable operating authority of the public right-of-way. In order to promote uniformity in style (community sense-of-place), shop drawings for all signage shall be submitted for Oshtemo Township approval prior to installation.

## **LAWN AND YARD RESTORATION**

After construction is completed all disturbed lawn areas, including adjacent cut and fill areas as required to blend into the existing yards, shall be repaired using a maximum of 1 on 4 back slope, four (4) inches of topsoil, MDOT Class A seed, fertilizer and mulch.

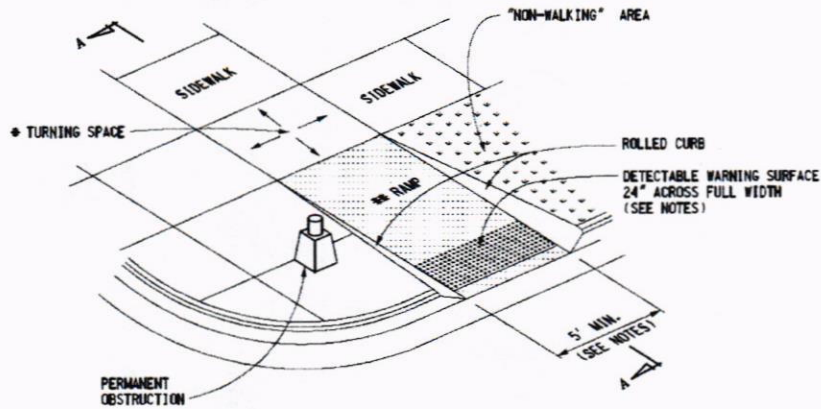
All disturbed areas, including adjacent cut and fill areas as required to blend into the existing yards, which are not lawns, shall be repaired using a maximum of 1 on 3 back slope, two (2) inches of topsoil, MDOT Roadside Seed, fertilizer and mulch.

Restored areas shall be repaired and reseeded as often as necessary in order to produce a close stand of weed free grass to the edges of the sidewalk or multi-use path.

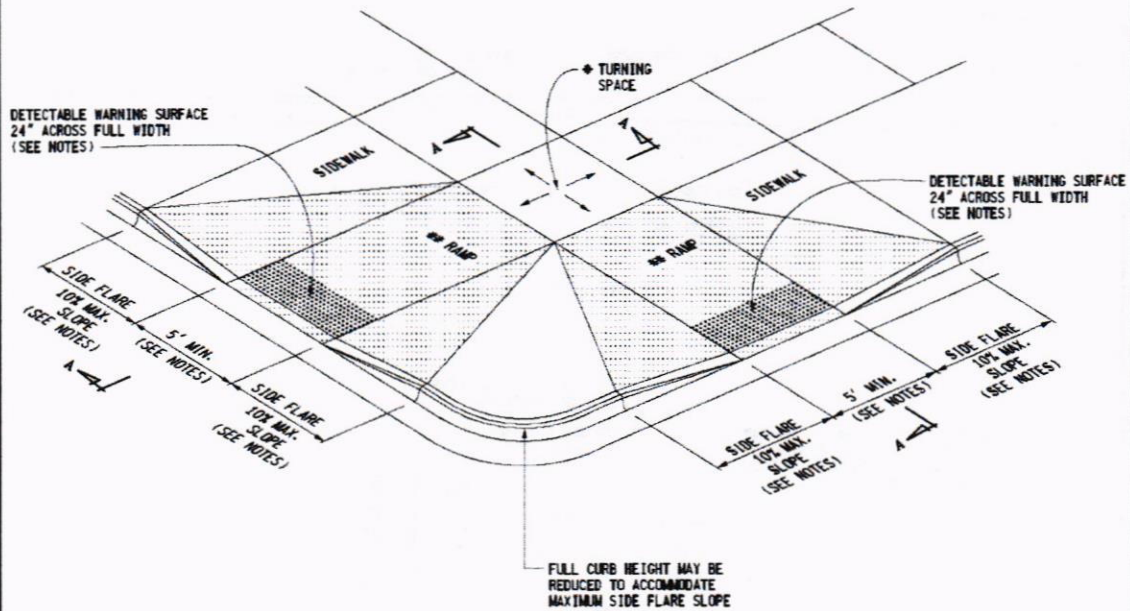
## **ATTACHMENTS:**

- MDOT Construction Details (2013 or current edition):
  - R-28-H Sidewalk Ramp and Detectable Warning Details
  - R-29-H Driveway Openings & Approaches and Concrete Sidewalk
- P&N Slope Cut & Fill Limitations
- P&N Precast Concrete Rip-Rap Retaining Wall
- P&N Lumber Retaining Wall
- P&N Lumber Fencing


- \* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.
- \*\* MAXIMUM RAMP CROSS SLOPE IS 2.0%. RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



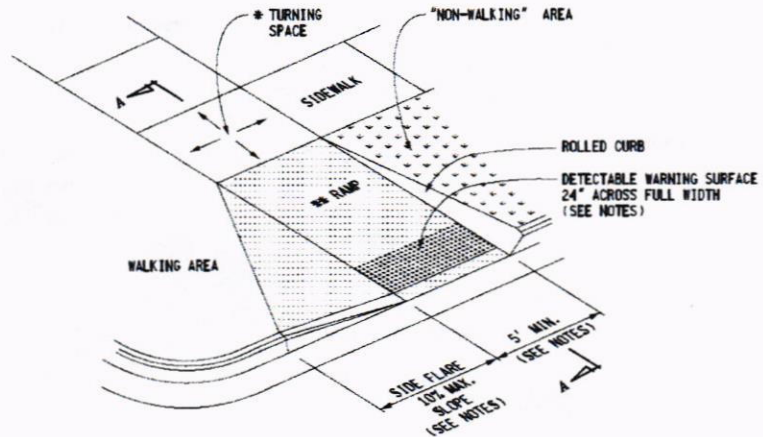
**SIDEWALK RAMP TYPE R**  
(ROLLED SIDES)



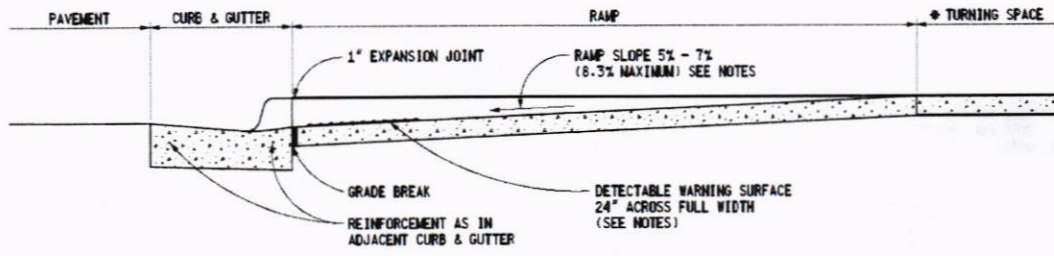
**SIDEWALK RAMP TYPE F**  
(FLARED SIDES, TWO RAMPS SHOWN)

 MICHIGAN DEPARTMENT OF TRANSPORTATION PREPARED BY DESIGN DIVISION DRAWN BY: <u>B.L.T.</u> CHECKED BY: <u>W.K.F.</u>	DEPARTMENT DIRECTOR Kirk T. Steudle APPROVED BY: <u>Randy V. Puffel</u> DIRECTOR, BUREAU OF FIELD SERVICES	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR <b>SIDEWALK RAMP AND          DETECTABLE WARNING DETAILS</b>	
	APPROVED BY: <u>Paul A. Van Pelt</u> DIRECTOR, BUREAU OF HIGHWAY DEVELOPMENT	1-25-2013 F.H.W.A. APPROVAL	10-3-2012 PLAN DATE

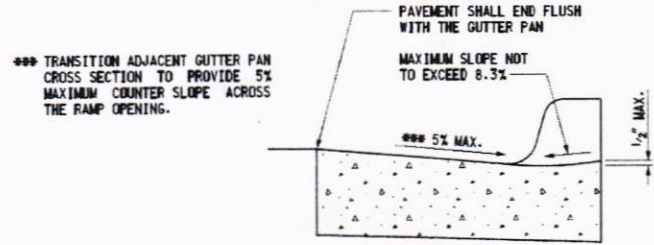
- \* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.
- \*\* MAXIMUM RAMP CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



**SIDEWALK RAMP TYPE RF**  
(ROLLED / FLARED SIDES)



**SECTION A-A**



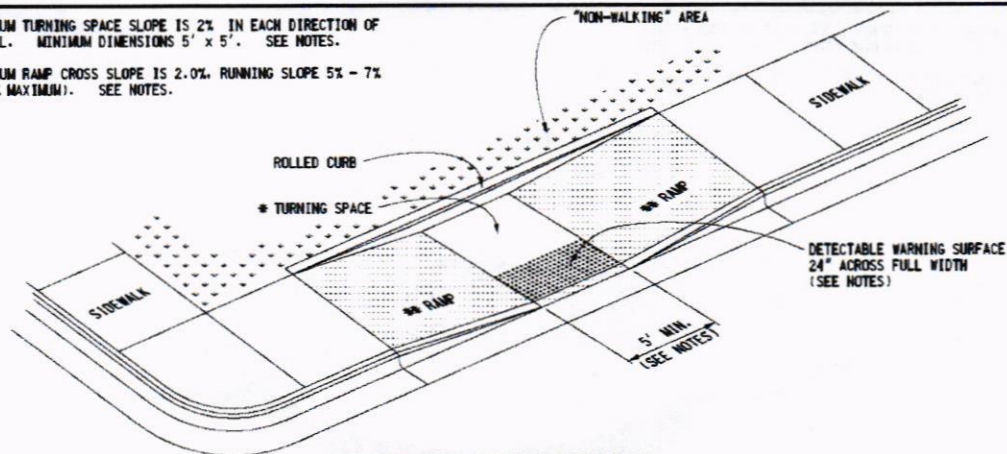
**SECTION THROUGH CURB CUT**  
(TYPICAL ALL RAMP TYPES)

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

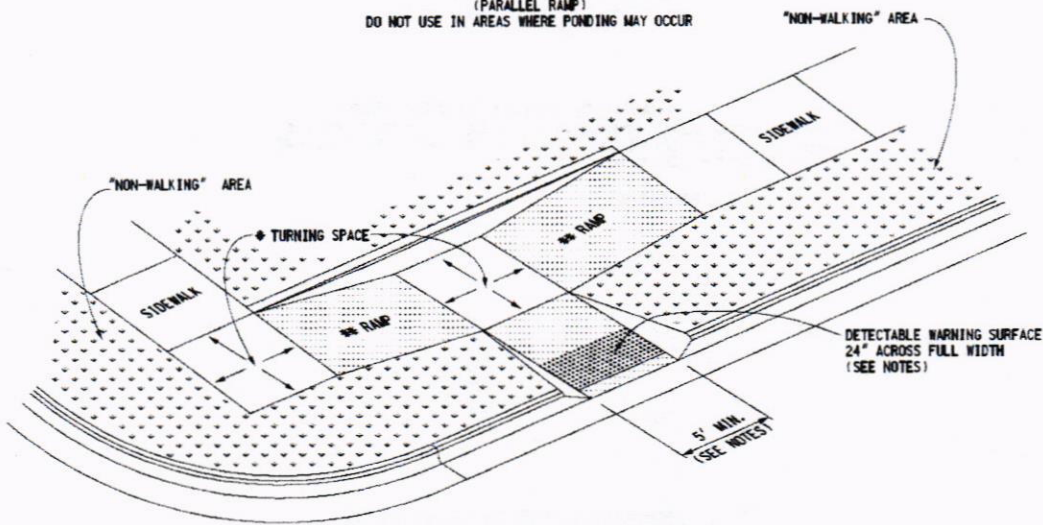
**SIDEWALK RAMP AND  
DETECTABLE WARNING DETAILS**

1-25-2013 F.H.V.A. APPROVAL	10-3-2012 PLAN DATE	R-28-H	SHEET 2 OF 7
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- \* MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.
- \*\* MAXIMUM RAMP CROSS SLOPE IS 2.0%. RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.

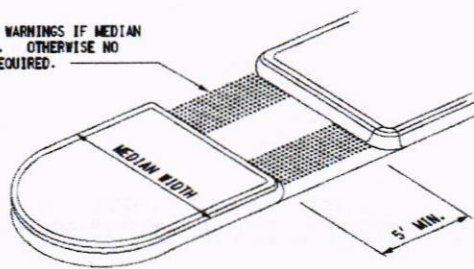


**SIDEWALK RAMP TYPE P**  
(PARALLEL RAMP)  
DO NOT USE IN AREAS WHERE PONDING MAY OCCUR



**SIDEWALK RAMP TYPE C**  
(COMBINATION RAMP)

USE 24" DEEP DETECTABLE WARNINGS IF MEDIAN WIDTH IS AT LEAST 6'-0". OTHERWISE NO DETECTABLE WARNING IS REQUIRED.



**SIDEWALK RAMP TYPE M**  
(MEDIAN ISLAND)

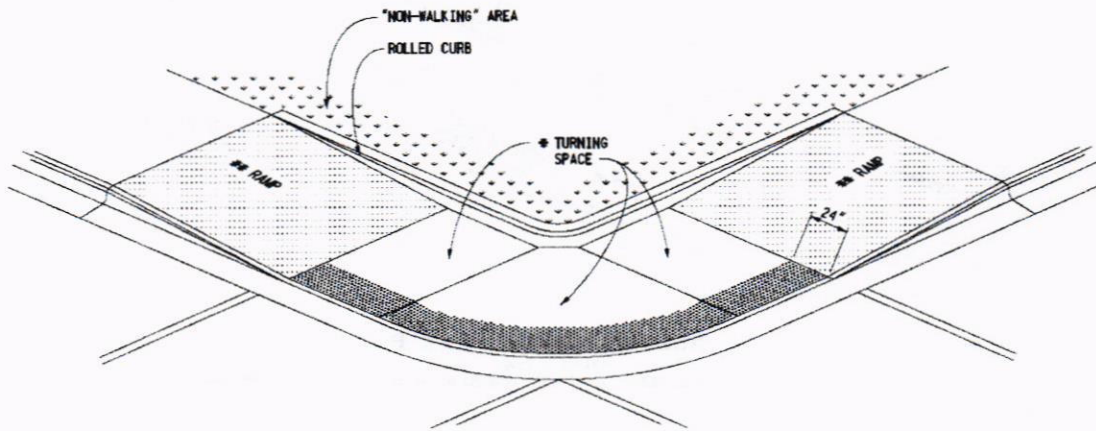
MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND  
DETECTABLE WARNING DETAILS**

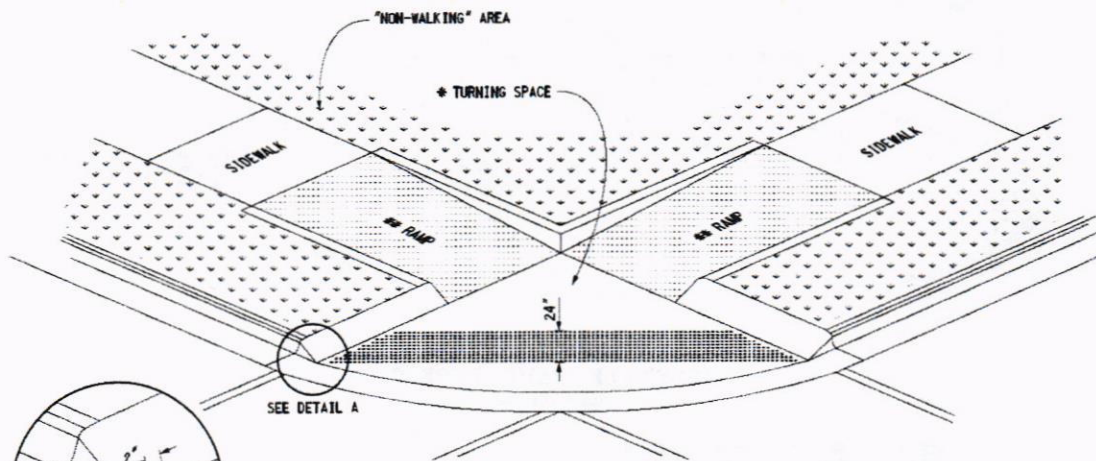
1-25-2013 F.H.V.A. APPROVAL	10-3-2012 PLAN DATE	R-28-H	SHEET 3 OF 7
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- ♦ MAXIMUM TURNING SPACE SLOPE IS 2% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE NOTES.
- ♦♦ MAXIMUM RAMP CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMUM). SEE NOTES.



( RADIAL DETECTABLE WARNING SHOWN )  
 DETECTABLE WARNING SURFACE COVERAGE IS 24" FOR THE FULL WIDTH OF THE RAMP OPENING EXCLUDING CURBED OR CURB TRANSITION AREAS. A CURB OFFSET NOT GREATER THAN 2" MEASURED ALONG THE EDGES OF THE DETECTABLE WARNING IS ALLOWABLE.



DETAIL A

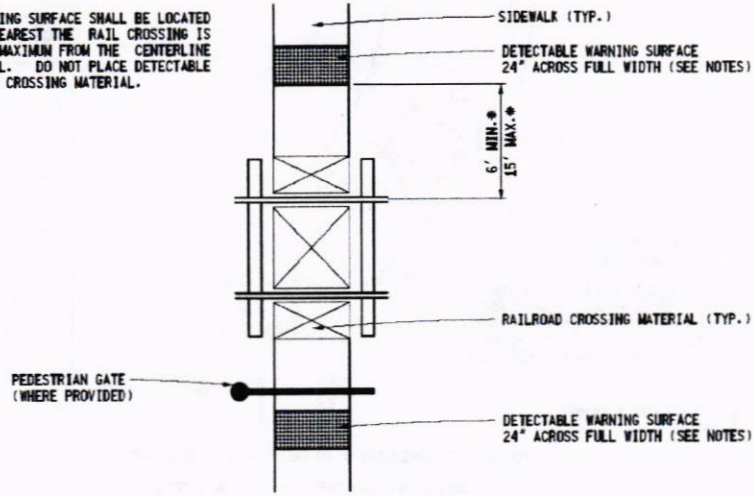
( TANGENT DETECTABLE WARNING SHOWN )  
 A CONCRETE BORDER OFFSET NOT GREATER THAN 2" IS ALLOWABLE FOR DETECTABLE WARNING INSTALLATION WHERE THE BACK OF CURB IS ON A RADIUS. THE BORDER OFFSET MAY BE MEASURED FROM THE END OF THE RADIUS.

**SIDEWALK RAMP TYPE D**  
 (DEPRESSED CORNER)

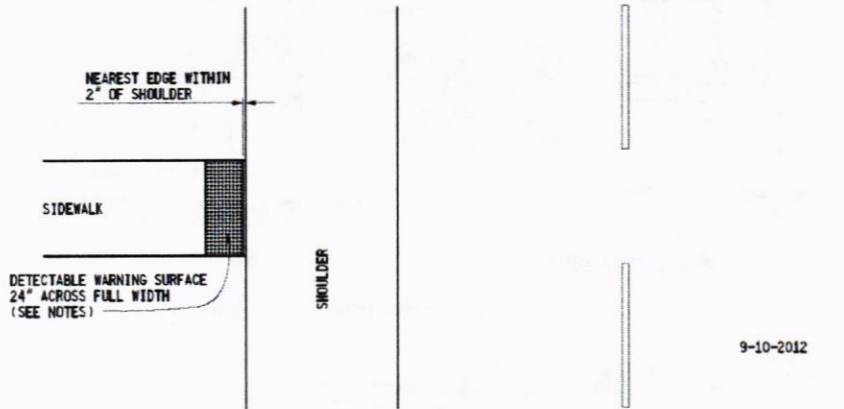
USE ONLY WHEN INDEPENDENT DIRECTIONAL RAMPS CAN NOT BE CONSTRUCTED FOR EACH CROSSING DIRECTION

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR			
<b>SIDEWALK RAMP AND DETECTABLE WARNING DETAILS</b>			
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\* THE DETECTABLE WARNING SURFACE SHALL BE LOCATED SO THAT THE EDGE NEAREST THE RAIL CROSSING IS 6' MINIMUM AND 15' MAXIMUM FROM THE CENTERLINE OF THE NEAREST RAIL. DO NOT PLACE DETECTABLE WARNING ON RAILROAD CROSSING MATERIAL.



DETECTABLE WARNING AT RAILROAD CROSSING

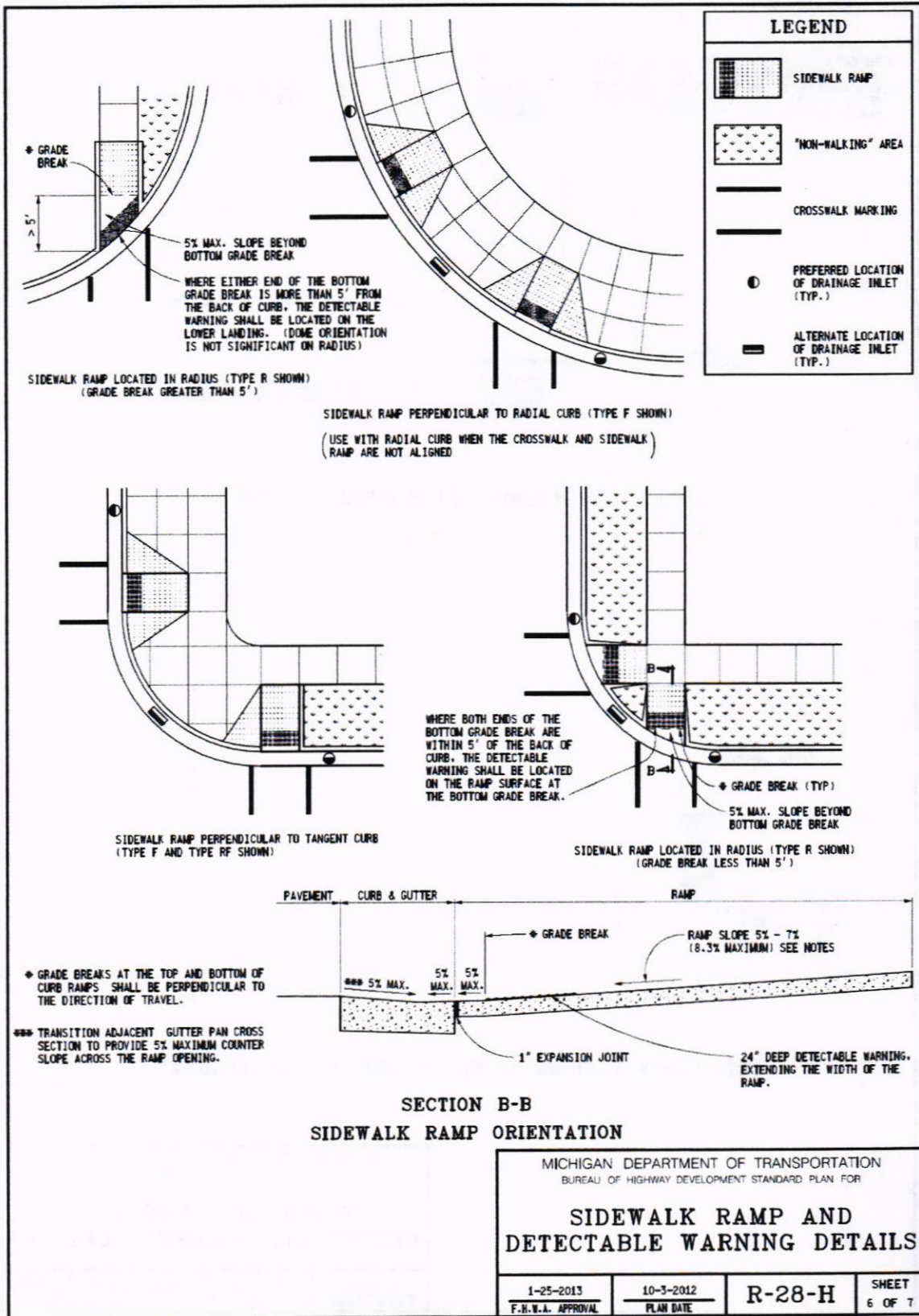


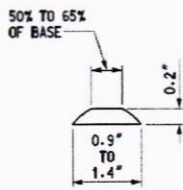
DETECTABLE WARNING AT FLUSH SHOULDER OR ROADWAY

MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

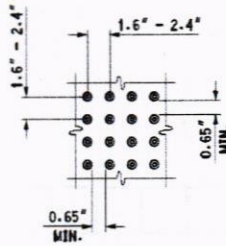
**SIDEWALK RAMP AND  
DETECTABLE WARNING DETAILS**

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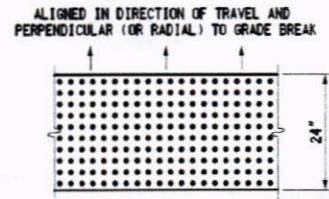




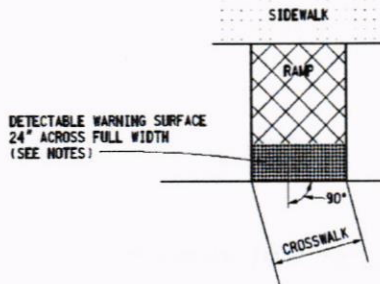
**DOME SECTION**



**DOME SPACING**



**DOME ALIGNMENT**



**DETECTABLE WARNING DETAILS**

**NOTES:**

DETAILS SPECIFIED ON THIS PLAN APPLY TO ALL CONSTRUCTION, RECONSTRUCTION, OR ALTERATION OF STREETS, CURBS, OR SIDEWALKS BY ALL PUBLIC AGENCIES AND BY ALL PRIVATE ORGANIZATIONS CONSTRUCTING FACILITIES FOR PUBLIC USE.

SIDEWALK RAMPS ARE TO BE LOCATED AS SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

RAMPS SHALL BE PROVIDED AT ALL CORNERS OF AN INTERSECTION WHERE THERE IS EXISTING OR PROPOSED SIDEWALK AND CURB. RAMPS SHALL ALSO BE PROVIDED AT MARKED AND/OR SIGNALIZED MID-BLOCK CROSSINGS.

SURFACE TEXTURE OF THE RAMP SHALL BE THAT OBTAINED BY A COARSE BROOMING, TRANSVERSE TO THE RUNNING SLOPE.

SIDEWALK SHALL BE RAMPED WHERE THE DRIVEWAY CURB IS EXTENDED ACROSS THE WALK.

CARE SHALL BE TAKEN TO ASSURE A UNIFORM GRADE ON THE RAMP. WHERE CONDITIONS PERMIT, IT IS DESIRABLE THAT THE SLOPE OF THE RAMP BE IN ONLY ONE DIRECTION, PARALLEL TO THE DIRECTION OF TRAVEL.

RAMP WIDTH SHALL BE INCREASED, IF NECESSARY, TO ACCOMMODATE SIDEWALK SNOW REMOVAL EQUIPMENT NORMALLY USED BY THE MUNICIPALITY.

PROVIDE TURNING SPACES WHERE PEDESTRIAN TURNING MOVEMENTS ARE REQUIRED.

WHEN 5' MINIMUM WIDTHS ARE NOT FEASIBLE, RAMP WIDTH MAY BE REDUCED TO NOT LESS THAN 4' AND TURNING SPACES TO NOT LESS THAN 4' x 4'.

DETECTABLE WARNING SURFACE COVERAGE IS 24" MINIMUM IN THE DIRECTION OF RAMP/PATH TRAVEL AND THE FULL WIDTH OF THE RAMP/PATH OPENING EXCLUDING CURBED OR FLARED CURB TRANSITION AREAS. A CURB OFFSET NOT GREATER THAN 2" MEASURED ALONG THE EDGES OF THE DETECTABLE WARNING (OR AS OTHERWISE SHOWN ON THIS STANDARD) IS ALLOWABLE.

FOR NEW ROADWAY CONSTRUCTION, THE RAMP CROSS SLOPE MAY NOT EXCEED 2%. FOR ALTERATIONS TO EXISTING ROADWAYS, THE CROSS SLOPE MAY BE TRANSITIONED TO MEET AN EXISTING ROADWAY GRADE. THE CROSS SLOPE TRANSITION SHALL BE APPLIED UNIFORMLY OVER THE FULL LENGTH OF THE RAMP.

THE MAXIMUM RUNNING SLOPE OF 8.3% IS RELATIVE TO A FLAT (0%) REFERENCE. HOWEVER, IT SHALL NOT REQUIRE ANY RAMP OR SERIES OF RAMPS TO EXCEED 15 FEET IN LENGTH.

DRAINAGE STRUCTURES SHOULD NOT BE PLACED IN LINE WITH RAMPS. THE LOCATION OF THE RAMP SHOULD TAKE PRECEDENCE OVER THE LOCATION OF THE DRAINAGE STRUCTURE. WHERE EXISTING DRAINAGE STRUCTURES ARE LOCATED IN THE RAMP PATH OF TRAVEL, USE A MANUFACTURER'S ADA COMPLIANT GRATE. OPENINGS SHALL NOT BE GREATER THAN 1/2". ELONGATED OPENINGS SHALL BE PLACED SO THAT THE LONG DIMENSION IS PERPENDICULAR TO THE DOMINANT DIRECTION OF TRAVEL.

TRANSITION THE GUTTER PAN CROSS SECTION SUCH THAT THE COUNTER SLOPE IN THE DIRECTION OF RAMP TRAVEL IS NOT GREATER THAN 5%. MAINTAIN THE NORMAL GUTTER PAN CROSS SECTION ACROSS DRAINAGE STRUCTURES.

THE TOP OF THE JOINT FILLER FOR ALL RAMP TYPES SHALL BE FLUSH WITH THE ADJACENT CONCRETE.

CROSSWALK AND STOP LINE MARKINGS, IF USED, SHALL BE SO LOCATED AS TO STOP TRAFFIC SHORT OF RAMP CROSSINGS. SPECIFIC DETAILS FOR MARKING APPLICATIONS ARE GIVEN IN THE "MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".

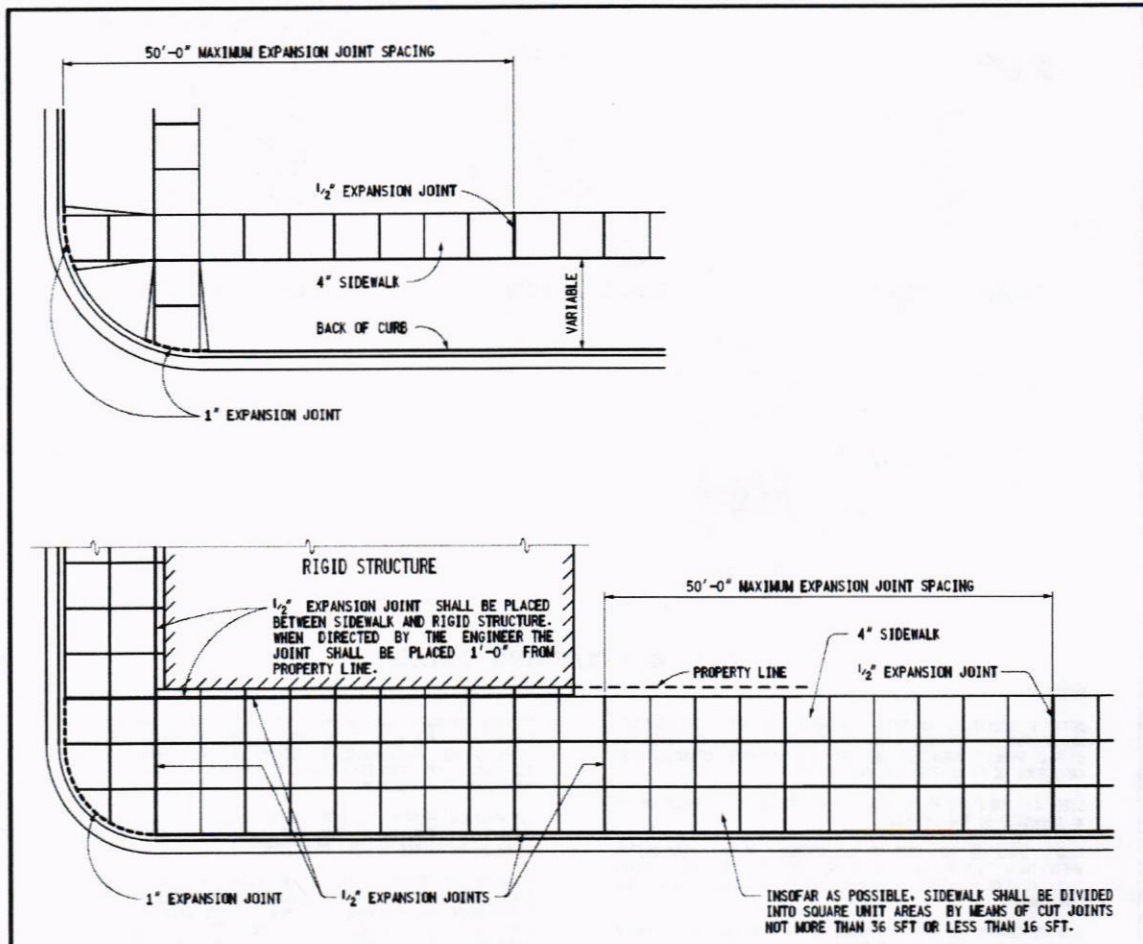
FLARED SIDES WITH A SLOPE OF 10% MAXIMUM, MEASURED ALONG THE ROADSIDE CURB LINE, SHALL BE PROVIDED WHERE AN UNOBSTRUCTED CIRCULATION PATH LATERALLY CROSSES THE SIDEWALK RAMP. FLARED SIDES ARE NOT REQUIRED WHERE THE RAMP IS BORDERED BY LANDSCAPING, UNPAVED SURFACE OR PERMANENT FIXED OBJECTS. WHERE THEY ARE NOT REQUIRED, FLARED SIDES CAN BE CONSIDERED IN ORDER TO AVOID SHARP CURB RETURNS AT RAMP OPENINGS.

DETECTABLE WARNING PLATES MUST BE INSTALLED USING FABRICATED OR FIELD CUT UNITS CAST AND/OR ANCHORED IN THE PAVEMENT TO RESIST SHIFTING OR HEAVING.

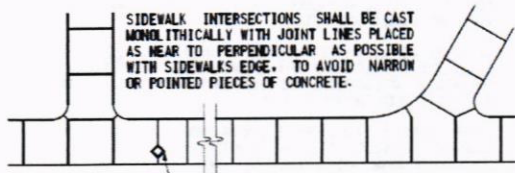
MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**SIDEWALK RAMP AND  
DETECTABLE WARNING DETAILS**

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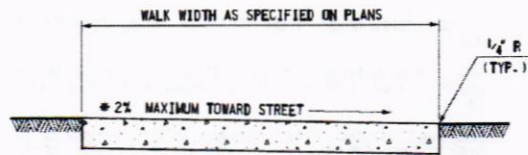


**LOCATION OF JOINTS IN CONCRETE SIDEWALK**



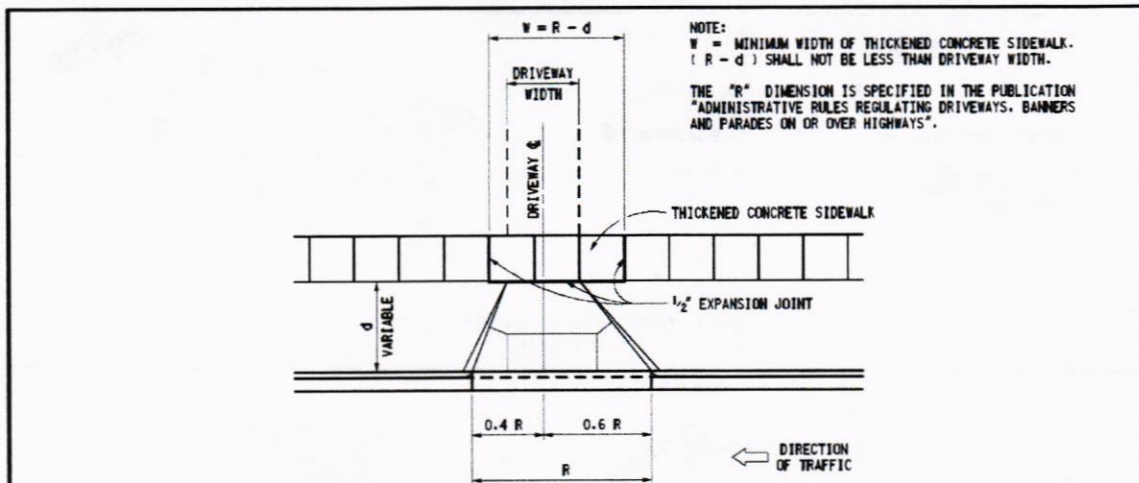
WHERE A PERMANENT STRUCTURE IS LOCATED IN SIDEWALK, PLACE EXPANSION MATERIAL AROUND STRUCTURE AND ADJUST JOINT PATTERN TO INTERSECT STRUCTURE AS ILLUSTRATED.

**TYPICAL SIDEWALK JOINT LAYOUTS**

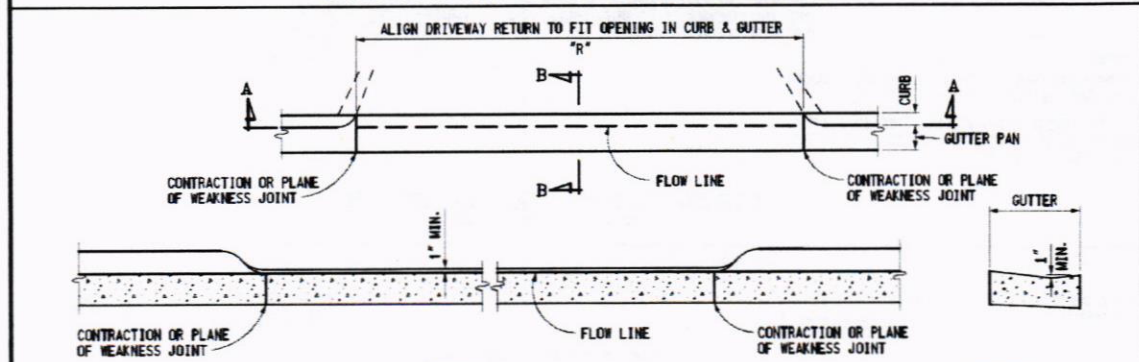


**4" CONCRETE SIDEWALK**

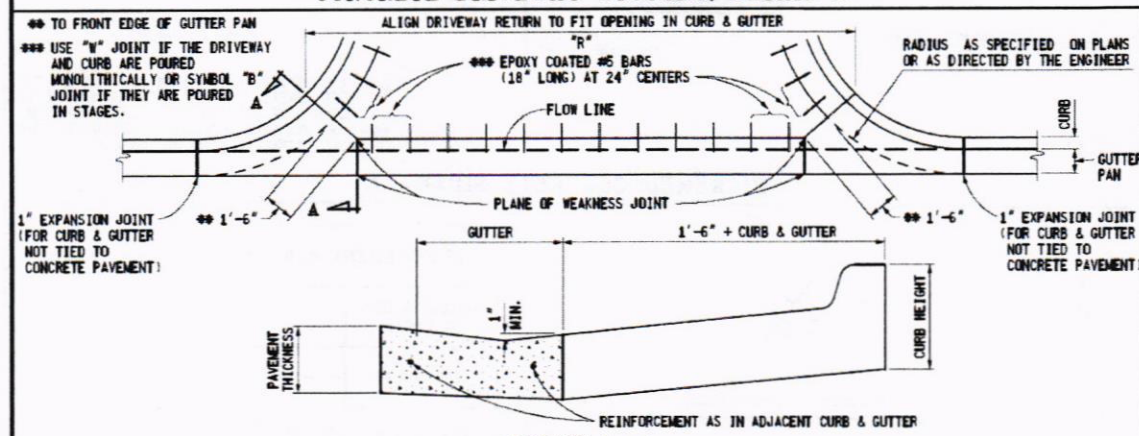
	DEPARTMENT DIRECTOR Kirk T. Steudle	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR	
	APPROVED BY: <i>Randy V. Pittler</i> DIRECTOR, BUREAU OF FIELD SERVICES	<b>DRIVEWAY OPENINGS          &amp; APPROACHES,          AND CONCRETE SIDEWALK</b>	
PREPARED BY DESIGN DIVISION	APPROVED BY: <i>Neal A. Van Pelt</i> DIRECTOR, BUREAU OF HIGHWAY DEVELOPMENT	1-25-2013 F.B.V.L. APPROVAL	10-1-2012 PLAN DATE
DRAWN BY: <i>B.L.T.</i>		<b>R-29-H</b>	SHEET 1 OF 4
CHECKED BY: <i>V.K.P.</i>			



**CONCRETE DRIVEWAY OPENING LAYOUT**



**CONCRETE DRIVEWAY OPENING, DETAIL L**



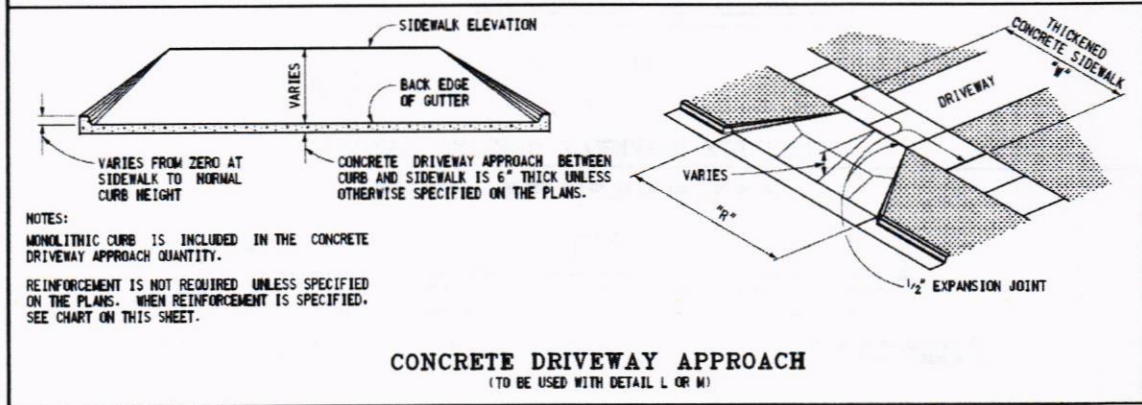
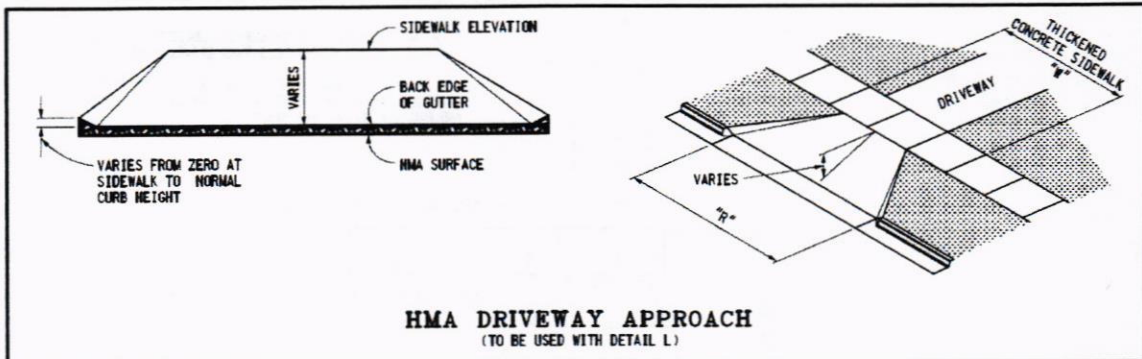
**CONCRETE DRIVEWAY OPENING, DETAIL M**

**NOTE:**  
 FOR ROADWAYS WITH CONCRETE PAVEMENTS, LONGITUDINAL LANE TIES WILL BE CONTINUOUS THROUGH THE DRIVEWAY OPENING AND THE SPACING OF THE #5 BARS IN CONCRETE DRIVEWAYS SHALL BE ADJUSTED TO AVOID CONFLICT WITH THE LONGITUDINAL LANE TIES.

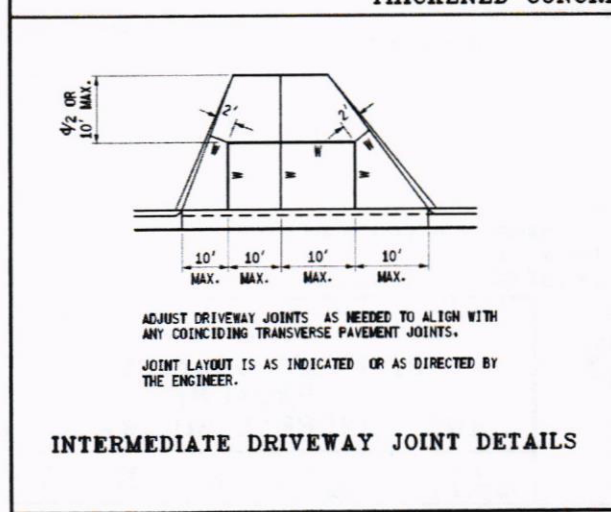
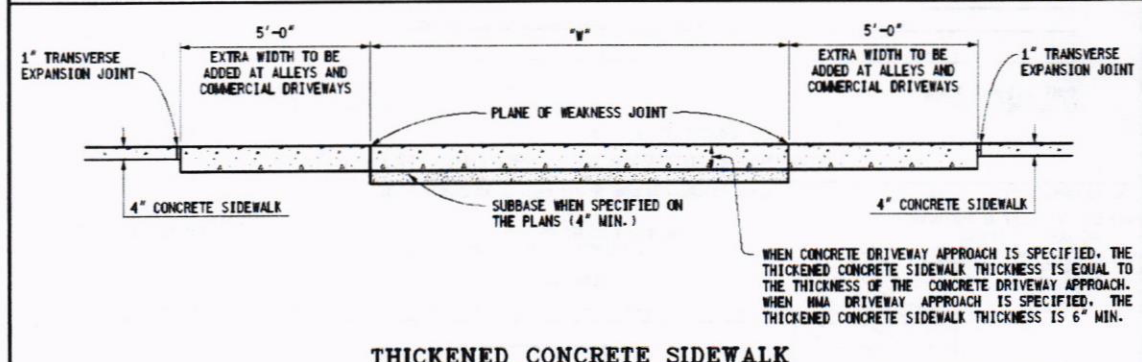
MICHIGAN DEPARTMENT OF TRANSPORTATION  
 BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**DRIVEWAY OPENINGS  
 & APPROACHES,  
 AND CONCRETE SIDEWALK**

1-25-2013 F.H.V.A. APPROVAL	10-1-2012 PLAN DATE	R-29-H	SHEET 2 OF 4
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NOTES:  
MONOLITHIC CURB IS INCLUDED IN THE CONCRETE DRIVEWAY APPROACH QUANTITY.  
REINFORCEMENT IS NOT REQUIRED UNLESS SPECIFIED ON THE PLANS. WHEN REINFORCEMENT IS SPECIFIED, SEE CHART ON THIS SHEET.

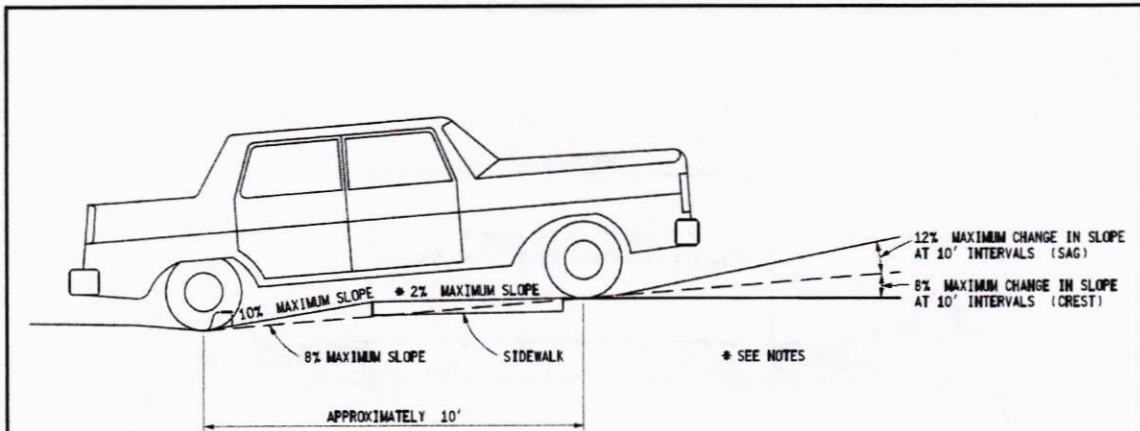


REINFORCEMENT FOR CONCRETE DRIVEWAYS		
CONCRETE DRIVEWAY THICKNESS	WIRE SIZE (6" x 6" MESH)	AVERAGE WEIGHT (LBS./100 SFT)
LESS THAN 8"	W1.4	21
	W2.9	42
8" OR GREATER	USE WIRE FABRIC REINFORCEMENT SPECIFIED ON STANDARD PLAN R-37-SERIES	

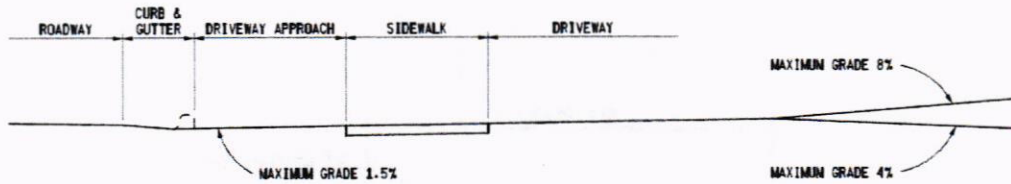
MICHIGAN DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

**DRIVEWAY OPENINGS  
& APPROACHES,  
AND CONCRETE SIDEWALK**

1-25-2013 F.B.V.A. APPROVAL	10-1-2012 PLAN DATE	R-29-H	SHEET 3 OF 4
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**LOW VOLUME COMMERCIAL OR RESIDENTIAL DRIVEWAY SLOPES**



**COMMERCIAL DRIVEWAY PROFILE FOR MAJOR TRAFFIC GENERATORS**

**NOTES:**

FOR DRIVEWAY DESIGN REFER ALSO TO "ADMINISTRATIVE RULES REGULATING DRIVEWAYS, BANNERS, AND PARADES ON OR OVER HIGHWAYS" AND GEOMETRIC DESIGN G-680-SERIES, COMMERCIAL DRIVEWAYS.

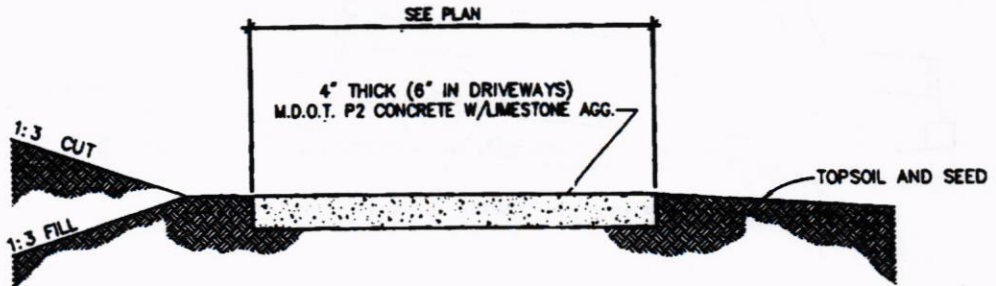
FOR CURB AND GUTTER DETAILS, SEE STANDARD PLAN R-30-SERIES.

TRANSVERSE SIDEWALK SLOPES ARE TYPICALLY 1.5% OR 2% MAXIMUM. IN ORDER TO MEET SITE CONDITIONS, IF THE TRANSVERSE SLOPE IS REQUIRED TO BE LESS THAN 1.5%, LONGITUDINAL DRAINAGE MUST BE PROVIDED.

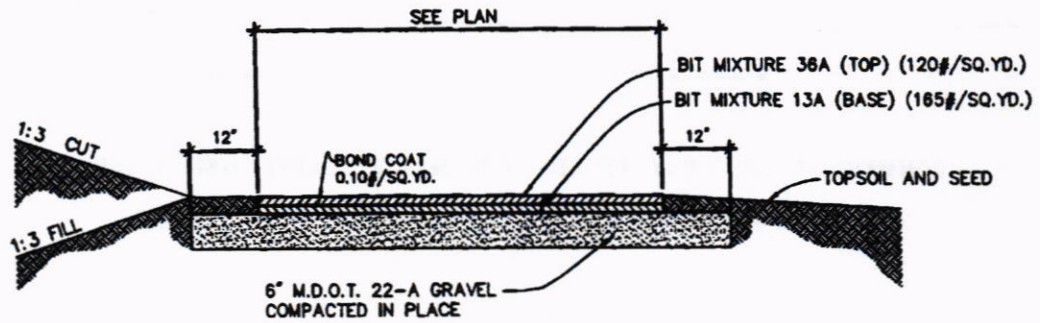
WHEN SETTING GRADES FOR COMMERCIAL DRIVES, THE TYPES OF VEHICLES USING THE DRIVE SHOULD BE CONSIDERED.

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR		
<b>DRIVEWAY OPENINGS &amp; APPROACHES, AND CONCRETE SIDEWALK</b>		
1-25-2013 F.H.V.A. APPROVAL	10-1-2012 PLAN DATE	R-29-H SHEET 4 OF 4





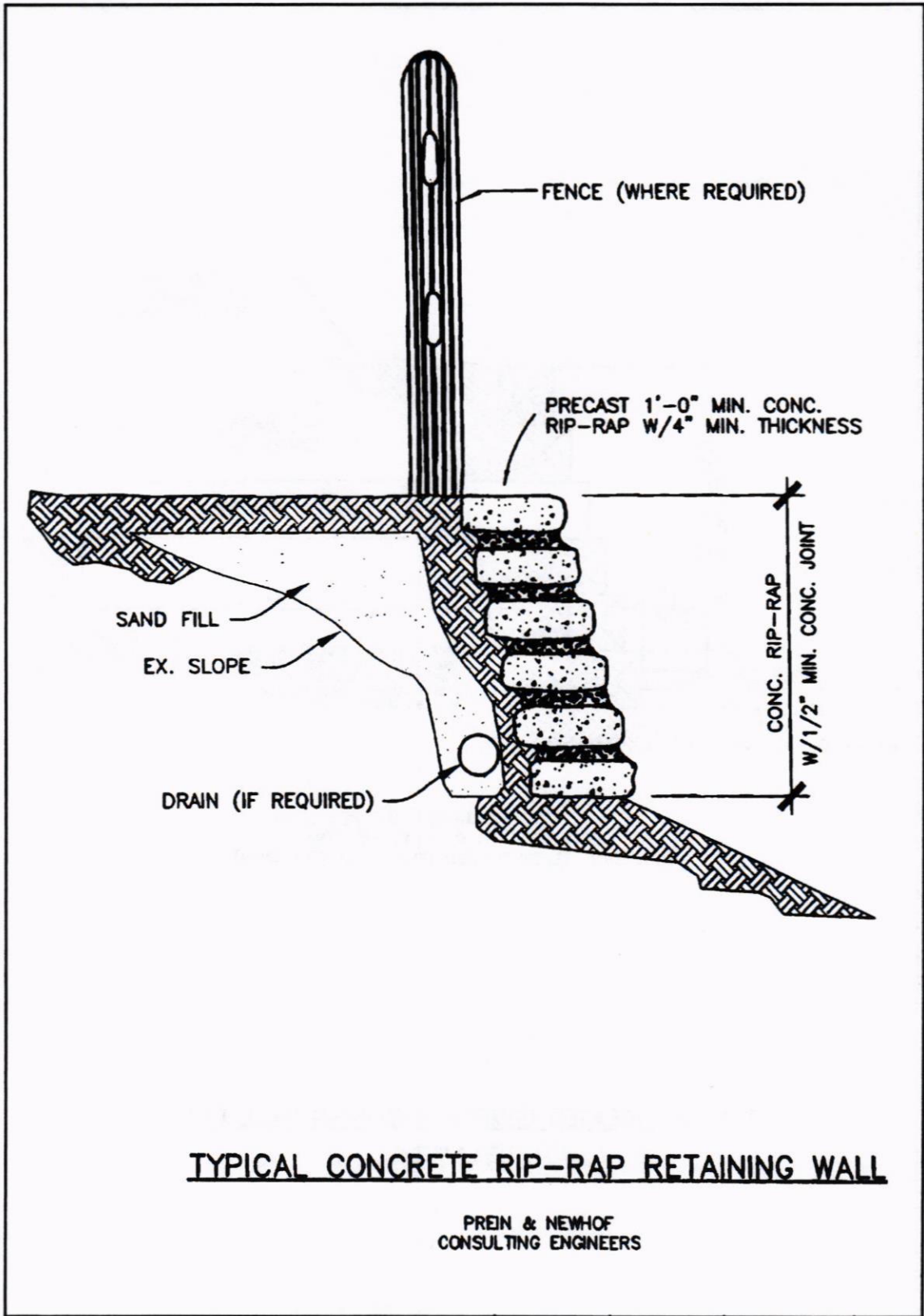
**CONCRETE SIDEWALK DETAIL**  
NO SCALE

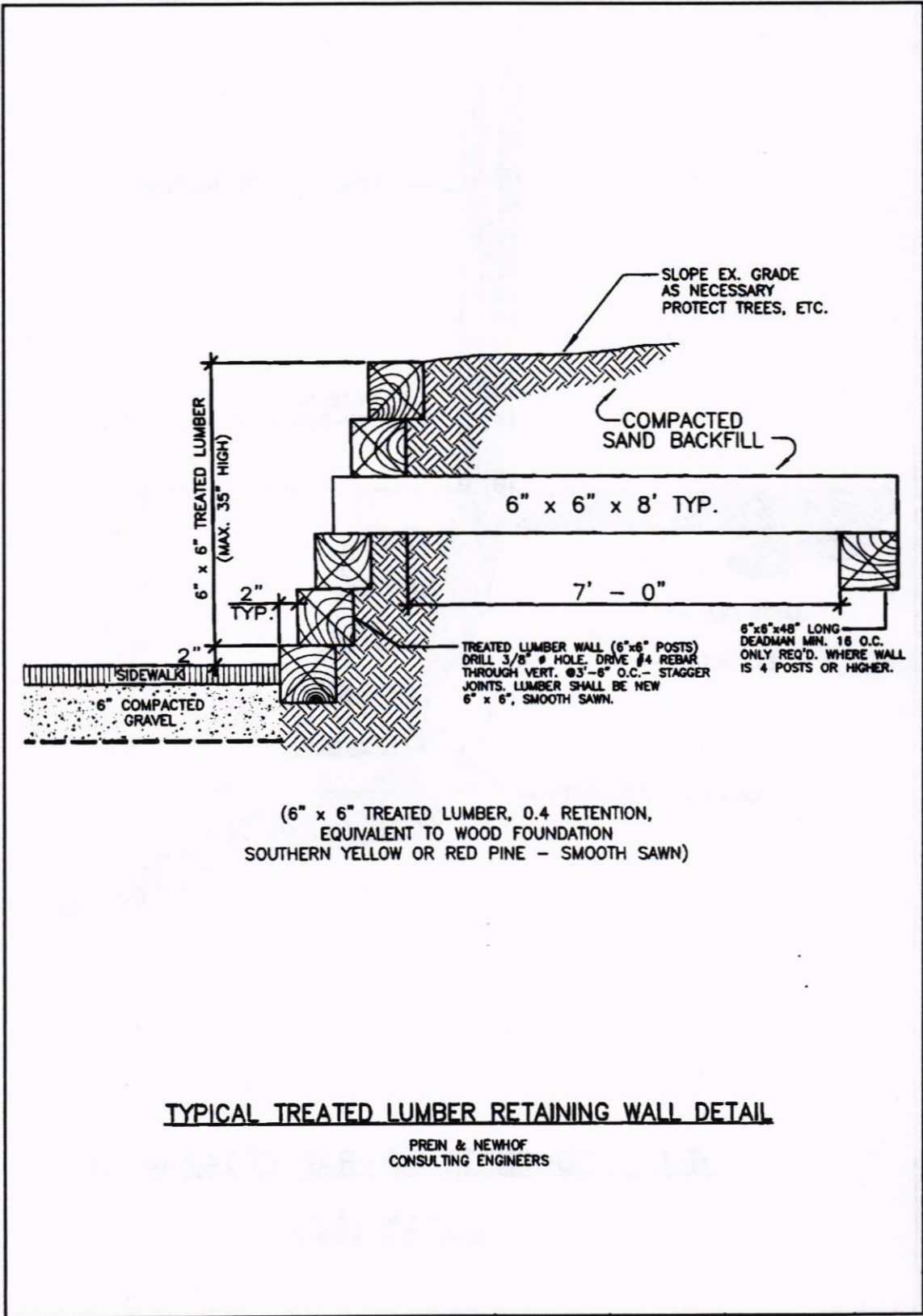


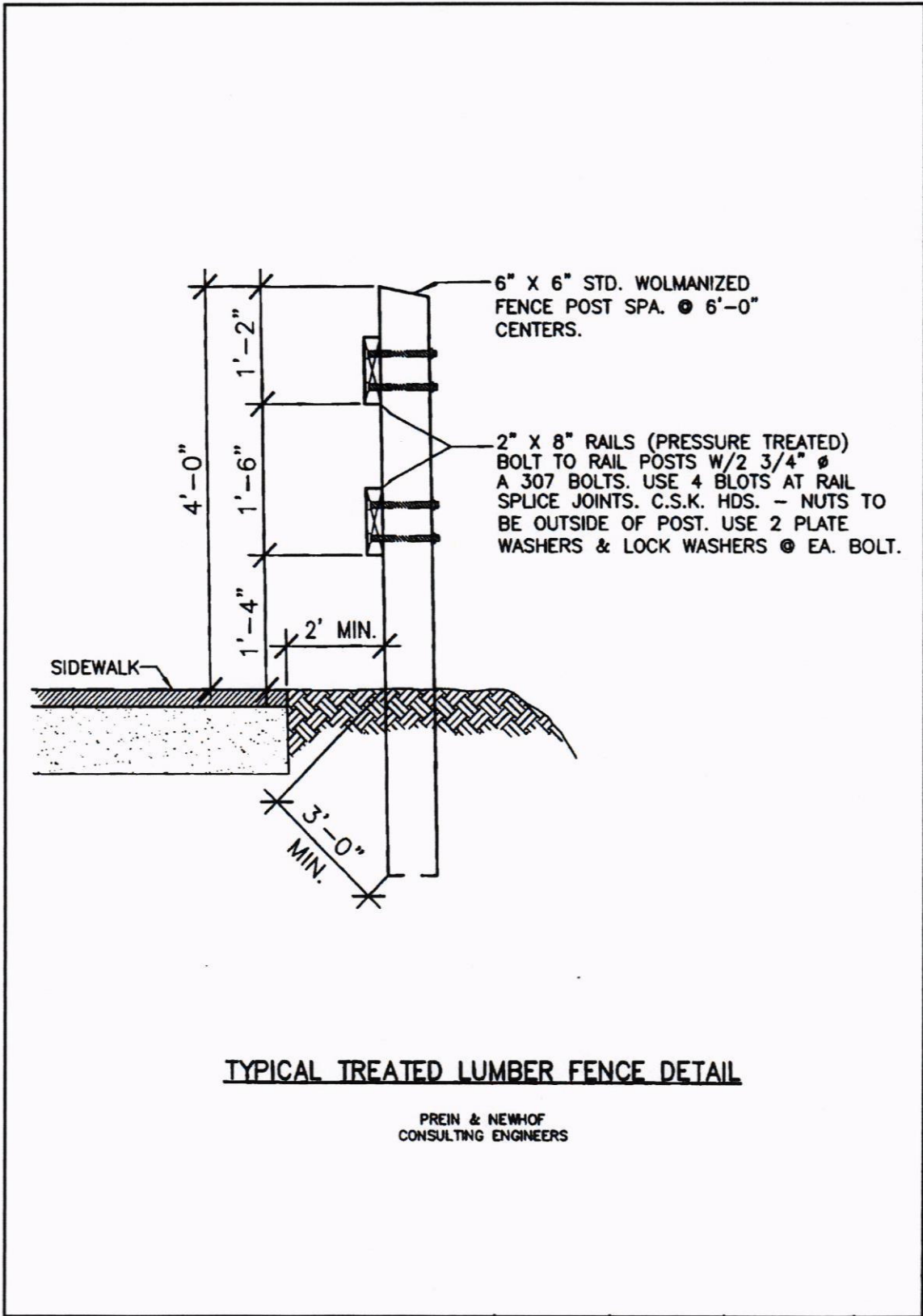
**BITUMINOUS MULTI-PURPOSE PATH DETAIL**  
NO SCALE

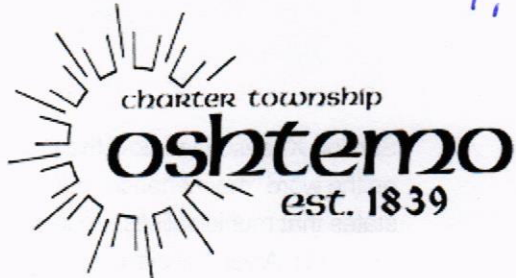
**SIDEWALK DETAILS**

PREIN & NEWHOF  
CONSULTING ENGINEERS









# Memo

**To:** Oshtemo Charter Township Board

**From:** Marc Elliott, P.E., Director of Public Works  
 Jamie Baker, Public Works Technical Specialist  
 Julie Johnston, AICP, Director of Planning

**Date:** August 24, 2018

**Subject:** Discussion on Complete Streets Policy Amendments

## OBJECTIVE

Present to the Board a revision to the draft Complete Streets Policy that was presented to the Board on August 14. These policy revisions are being developed to establish a policy mechanism to assist the Board in deciding where, when and how sidewalks and bike paths will be built within the Township. Additionally, give the Board the option to adopt the Complete Streets Policy at the August 28 or September 11 Board meeting.

## BACKGROUND

At the August 14 Board meeting, the Board asked for the following changes to be made to the draft Complete Streets policy:

- Revise Section VII to be consistent with existing Michigan laws regarding petitions and counter-petitions.
- Revise Section XI to clarify what is meant by "sidewalk maintenance."
- Provide a draft version of Appendix A: "Prioritization Policy" so that the Board can understand what a scoring system might look like.
- Provide a draft version of Appendix B: "Road Assessment Policy" so that the Board can understand how road and sidewalk special assessment district policy may have to change based on this Complete Streets policy.
- Provide a cost estimate to build sidewalks, clear snow from sidewalks, replace small sections of sidewalks, and major reconstruction of sidewalks.
- Other minor changes were made based on comments from residents, Township staff, and our non-motorized consultant, The Greenway Collaborative.

## DISCUSSION

Attachment 1 is Revision 1 to the draft Complete Streets Policy. Section VII was revised, with the assistance of the Township Attorney, to be consistent with existing Michigan Statutes regarding petitions and counter-petitions for infrastructure improvements. Because the petition requirements are in the Michigan statutes, they are not repeated in the Complete Streets Policy. That also allows the Oshtemo Complete Streets Policy to be consistent with Michigan law regardless of changes made to the Michigan statutes.

Section XI describes how the Complete Streets Policy will be implemented. The Board asked for clarification on the word "maintenance" because that could refer to repairs and snow removal. Michigan Statute 691.1402a states that municipalities are liable for injuries occurring on sidewalks if one of two conditions apply:

(1) A vertical discontinuity defect of 2 inches or more in the sidewalk.

(2) A dangerous condition in the sidewalk itself of a particular character other than solely a vertical discontinuity.

Therefore, it is necessary to repair the sidewalks before these two conditions arise. These conditions were added to the Complete Streets Policy to define when maintenance would occur. Lastly, the word maintenance was changed to repair to improve clarity. Discussion on snow removal will occur in the cost estimating section below.

Appendix A is a draft scoring system to prioritize the sidewalk construction. The 5-year Capital Improvement Plan targets Drake Rd, Stadium Drive, KL Ave, 9<sup>th</sup> Street, and 11<sup>th</sup> Street as the highest traffic roads requiring sidewalks or bike paths. As these roads are completed, the Township will need a process for determining the next roads to receive sidewalks. The draft scoring system provides a framework for prioritizing sidewalk construction. This appendix requires more work to figure out a scoring system that provides logical results. Appendix A does not have to be approved with the Complete Streets Policy. As sidewalks are built on the major roads above, we will need to start planning for the next streets in a year or two.

Appendix B is a revision of an existing Township policy for road, sidewalk, and shared-use path assessments (if any). The policy states that the Township will pay for chip seals and hot-mix asphalt (HMA) overlays. However, if the residents of a street want to upgrade to an ultra-thin HMA overlay instead of the chip seal then the residents will be assessed for the difference between the two treatments. The current policy also states that residents will be assessed \$30 per foot of sidewalk and commercial/industrial zoned properties will be assessed \$35 per foot. The road and sidewalk assessment policy needs to be revised to be consistent with the rest of the Complete Streets Policy. It also made sense to incorporate the assessment policy into the Complete Streets Policy so that there was only one document covering all aspects of roads and sidewalks.

The cost to build non-motorized facilities will be discussed as part of the agenda item for sidewalks with sewer. We are still awaiting cost estimates from sidewalk contractors for routine sidewalk repairs.

Lastly, the Board asked about the cost to clear snow from sidewalks and bike paths. The Township currently plows the 10-foot bike path along both sides of West Main Street between US-131 and the Township Hall. The snow removal analysis assumed snow removal from sidewalks and bike paths on only the highest traffic streets (West Main, Drake Rd, Stadium Drive, 9<sup>th</sup> Street, and KL Ave) and in front of Township properties. The analysis of the costs is in Attachment 2.

## **ATTACHMENTS**

1. Draft Oshtemo Complete Streets Policy Revision 1 (including Future Land Use Map)
2. Annual Snow Removal Cost Estimate

## **OSHTEMO TOWNSHIP COMPLETE STREETS POLICY**

### **I. THE COMPLETE STREETS INTENT**

Oshtemo Township, through this Complete Streets Policy and the adopted Master Plan, recognizes that planning the Township transportation system involves more than just moving vehicles efficiently and safely. A transportation system needs to meet the needs of all types of users – motorists, pedestrians (including those with disabilities), bicyclists, transit vehicles and users, freight haulers, emergency responders, and citizens of all ages and abilities. Additionally, the Vision Zero traffic safety project establishes a goal of eliminating traffic deaths and severe injuries by improving the design of the transportation system.

For many years, Oshtemo Township has planned and implemented a multi-modal transportation system to safely and efficiently move both motorists and non-motorists throughout the community. Furthermore, the design of the multi-modal system has also considered the context of the adjacent land uses. Comprehensive planning, design, and construction efforts that consider all users as part of a transportation system will continue as public and private developments and capital improvements are implemented.

### **II. BENEFITS OF A COMPLETE STREETS TRANSPORTATION SYSTEM**

Continuing to provide and invest in a Complete Streets system is a priority for residents, businesses and Township leaders, and includes many benefits, such as:

- Provides multi-modal transportation connections between homes, schools, parks, public transportation, offices, and retail destinations.
- Enhances safe walking and bicycling options for school-age children, consistent with the national Safe Routes to Schools program
- Encourages economic growth, positively impacting property values.
- Improves pedestrian and cyclist safety by reducing potential crashes between motorized and non-motorized users.
- Encourages walking and bicycling that improves health and fitness.
- Provides driving alternatives positively impacting the environment, conserves fuel, and saves money.

### **III. APPLICABILITY OF THE COMPLETE STREETS POLICY**

Oshtemo Township will consider every transportation improvement and private development project as an opportunity to evaluate the level of Complete Streets elements within the general project area and connectivity to adjacent areas with the intent to improve safety and accessibility for all users. This Complete Streets Policy will be used with all planning, design, funding, and approval involving any street construction, reconstruction, resurface, major repair or alteration, or right-of-way acquisition project. This policy is also applicable to the installation, replacement, or reconstruction of underground utilities located within a public street right-of-way; however, water and sanitary sewer funds cannot be used as a Complete Streets funding source.

A determination as to whether or not a specific street is a Complete Street, or how many design elements are appropriate will be determined on a case-by-case basis by Township Board.

#### IV. DESIGNING A MULTI-MODAL SYSTEM TO MEET THE NEEDS OF ALL USERS

Oshtemo Township will seek to enhance the safety, access, convenience, and comfort for all users of all ages and abilities through the planning, design, operation, enforcement, and maintenance of the transportation network. The goal is to continue to expand an interconnected network of facilities accommodating each mode of travel consistent with the goals identified in the adopted Master Plan and other related planning documents. Furthermore, this policy recognizes that all streets are not the same: Streets vary by type and intensity of adjacent land uses, number of travel lanes, posted speed limits, traffic volumes, available right-of-way, and other characteristics such as topography, soil types, and proximity of trees. As a result, the necessity for certain non-motorized transportation facilities will need to be carefully evaluated such that the cost of the non-motorized facility is balanced with the demand and availability of right-of-way.

Transportation improvements will include facilities and other amenities that are recognized as contributing to Complete Streets, which may include, but not necessarily limited to, one or more of the following:

1. Sidewalks (new construction, gap construction, repair or replacement, and Americans with Disabilities Act (ADA) improvements).
2. Pedestrian refuge islands or crosswalk improvements.
3. Traffic calming measures, where appropriate.
4. Street and/or sidewalk lighting.
5. Multi-use trails.
6. Accessibility improvements consistent with the ADA.
7. Improved pedestrian access and amenities (e.g. bus shelters) to transit stops and/or installation of bus turnouts.
8. Bicycle accommodations including designated bike lanes, widened travel lanes, and bike storage facilities.
9. Connecting sidewalks along a public street to internal private development sidewalks.
10. Interconnected and/or shared off-street parking lots and other access management techniques to reduce driveways and subsequent points of conflict between vehicles and pedestrians/bicyclists.

#### V. CONTEXT SENSITIVE STREET DESIGN

Historically, streets were designed to meet expected traffic volumes at a selected speed. Streets or intersections were widened to meet vehicular needs provided right-of-way was available. That traditional approach has been changed in response to best practices in street design, which recognize that many factors along a particular street influence the speed of traffic, crashes, and how a street can be in harmony, or in conflict, with its surroundings.

Street width, presence of on-street parking and sidewalks, block length, building setbacks, design speed, pavement markings and signs, street trees, and even boulevard landscaping, and street furniture all contribute to how the street functions. Driver perceptions can affect vehicle speed and care used in driving. These elements can also affect how people view a corridor and their impression of Oshtemo in general. A street designed in tandem with its surroundings can foster an inviting place to live, work and visit.

Oshtemo Township will continue to rely on a wide range of concepts to help ensure the transportation system operates safely and efficiently, but also in context with the immediate area and overall character of the Township.



## VI. IMPROVE NETWORK CONNECTIVITY

Improving the connectivity between the current network of sidewalks, bike lanes, multi-use trails, paths, public transit routes, and roadways is important to ensure the continued success of complete streets concepts in Oshtemo Township. A well-connected network will encourage citizens to fully utilize all available forms of transportation that will provide safe access for all users, promote healthy living, increase the capacity and efficiency of the roadway network, and reduce negative environmental impacts. It is also important that a well-connected network include safe and convenient transitions from one mode of transportation to another, from one jurisdiction to another and from one type of infrastructure improvement to another. This can be accomplished by including ADA-compliant improvements, appropriate signage/pavement markings, refuge islands, sidewalk connections to bus stops, among others.

## VII. NON-MOTORIZED COMPLETE STREET ELEMENTS

The Non-Motorized Facilities section of the Oshtemo Township Master Plan provides specific recommendations concerning the provision of the following non-motorized Complete Street elements:

1. Using the Future Land Use Map from the Master Plan, in areas identified as Rural Residential and Neighborhood Commercial, a widened-shoulder bikeway on major thoroughfares will be built to accommodate bicycle travel.
2. Sidewalks (that are ADA-compliant) will be built on local and major streets as follows:
  - a. Due to high traffic volume spanning the Rural Residential and more developed portions of the Township, ~~sidewalks or bike paths~~ non-motorized facilities will be built along portions of M-43 and Stadium Drive consistent with the Oshtemo Township Non-Motorized Facilities Plan.
  - b. Using the Future Land Use Map from the Master Plan, all areas not identified as Rural Residential or Neighborhood Commercial, 6-foot sidewalks or 10-foot bike-shared-use paths are planned on all major thoroughfares, defined as Average Daily Traffic greater than 1,000 vehicles per day.
  - c. Five-foot sidewalks are provided on both sides of all streets within new residential neighborhoods.
  - d. Sidewalks-Non-motorized facilities within existing residential neighborhoods, commercial areas, and industrial areas are inconsistent throughout the community. In such areas, the following criteria should be considered for the installation of sidewalks or shared-use paths:
    - i. Existing residential neighborhoods without sidewalks, and for which sidewalks are not planned under the Township Master Plan, may petition for sidewalks to be built within the neighborhood under any Michigan Public Act which would allow for the installation of public sidewalks decide whether they want sidewalks based on majority petition. All properties get one vote. Majority is defined as greater than 50% of the properties. The residents of the neighborhood will be responsible for initiating the petition and, if successful, present the petition to the Township Board for approval. Upon Township Board approval, the streets of the neighborhood will be added to the list of streets to receive sidewalks and the sidewalks will be built at Township expense according to the Appendix A prioritization policy. If the residents want the sidewalks

**Commented [JB1]:** The different Public Acts vary in their requirements and describing all of them in this document would be difficult.

**Commented [JB2]:** Inconsistent with existing Michigan Law. Number of feet of road frontage to be used.

**Commented [JB3]:** Majority is defined in the specific Michigan Public Act used for the petition.

sooner than the residents can petition to establish a special assessment district to pay for the sidewalk;

- ii. The Township, on the motion of the Board, may, at its expense, proceed with sidewalk improvements to fill gaps in the existing sidewalk system as redevelopment and infill development occurs;
- iii. The Board may, concurrent with capital improvement projects, construction of the sidewalks, other non-motorized facilities, and other appropriate safety improvements along primary pedestrian routes to schools, parks, or other activity destinations should be prioritized upon motion of the Board at the Township's expense. A majority of properties can counter-petition the proposed sidewalks. The Township Board will consider the counter-petition per the applicable State of Michigan statutes regarding petitions. The Township will host public meetings to obtain resident input before committing to the Complete Street elements with the capital improvement projects.
- iii.iv. Within commercial and industrial areas, non-motorized facilities deemed necessary may be installed upon the motion of the Board in accordance with Michigan law with or without the establishment of a special assessment district.

**Commented [JB4]:** The process for countering the sidewalks is covered in the applicable Michigan statutes. Repeating them here is unnecessary.

3. For completeness, off-road bike-shared-use paths will be built per the Non-Motorized Facilities Plan.

#### VIII. EXCEPTIONS

Exceptions to this policy may be appropriate when one or more of the following conditions exists:

1. An affected roadway prohibits, by law, specified users (such as an interstate freeways or pedestrian malls), in which case a greater effort shall be made to accommodate those specified users elsewhere.
2. An alternative route, such as a multi-use trail in the immediate vicinity provides an option for non-motorized transportation.
3. A wide shared-shoulder bikeway designed to accommodate both bicycle and pedestrian use is available in the street right-of-way.
4. There is insufficient right-of-way to accommodate a Complete Street element at that time. The Township should attempt to acquire the right-of-way to accommodate the Complete Street element.
5. The activities, such as spot repair or crack seal, are ordinary maintenance activities designed to keep assets in serviceable condition, which meet MDOT's ADA upgrade exceptions.
6. Capital preventive maintenance projects where the roadwork is not substantial enough to recognize costs savings by including the non-motorized element. Examples of preventive maintenance projects include thin overlays (less than 1.5-inch), micro-seals, chip seals, or slurry seals.
7. The type of project does not have a substantial impact on the roadway, such as signal upgrades, addition of turning lanes and utility installations signal interconnect projects, which do not remove existing non-motorized or transit facilities.
8. Safety projects which are funded only for specific safety features identified by crash patterns, due to the funding parameters of the program.
9. The adopted Future Use Plan, or other present and/or anticipated market conditions that can be

documented, indicate an absence of existing or future need (e.g. sparse population), for a Complete Street element or elements.

10. An element of this policy will have an adverse impact on existing environmental resources such as wetlands, floodplains, creeks, or historic structures, or the presence of an environmental resource will add significant cost to an element of this policy.

Exceptions to this policy involving private (re)development projects requiring plan review approval or recommendation to Township Board by the Planning Commission shall only be granted or recommended when the Planning Commission, after recommendation from the Township Planning Staff, determines one or more of the above applicable provisions exist, is consistent with applicable sections of the Zoning Code concerning Planning Commission review/approval authority and is consistent with the goals, objectives and recommendations of the Master Plan.

#### IX. INTERGOVERNMENTAL COOPERATION

Oshtemo Township will cooperate with adjacent communities, [the Road Commission of Kalamazoo County](#), the Kalamazoo Area Transportation Study, [Metro Transit](#), and Kalamazoo County to ensure the principles and practices of complete streets concepts are consistent in appropriate planning documents and implanted along the mutually shared corporate boundaries. Oshtemo Township, together with its adjacent neighbors, Kalamazoo Area Transportation Study and Kalamazoo County, will cooperate to make sure the transportation network as it transitions from one community to the next is seamless in accordance with local and regional road, transit, bicycle, and pedestrian plans and design criteria.

#### X. DESIGN GUIDELINES

Oshtemo Township will consider design guidelines promulgated in the documents listed below or as may be required by the funding source. Sources for design guidelines include, but are not limited to:

1. American Association for State Highway and Transportation Officials: A Policy on Geometric Design of Highways and Streets, 6th Edition, or latest edition.
2. American Association for State Highway and Transportation Officials: Guide for the Planning, Design, Operation of Pedestrian Facilities – July 2004 edition or latest edition.
3. American Association for State Highway and Transportation Officials: Development of Bicycle Facilities – Fourth Edition or latest edition.
4. Institute of Transportation Engineers: Designing Walkable Urban Thoroughfares: A Context Sensitive Approach – 2010 edition or latest edition.
5. [National Association of City Transportation Officials: Urban Street Design Guide](#)
- 4-6. [Federal Highway Administration: Complete Streets Design Guide](#)

In conjunction with the references noted above, the Township will also consider the impact on a street and any proposed improvements will have on surrounding land uses pursuant to Section V.

#### XI. IMPLEMENTATION

Oshtemo Township will take the following actions to implement this Complete Streets policy:

1. The Township will pay for construction, [maintenance/repairs](#), and reconstruction of all sidewalks [and shared use paths](#) in the road right-of-way that are not part of a new development

- (residential or commercial) from general revenue funds.
2. For commercial and industrial areas, the Township will pay for the construction, repairs, and reconstruction of all sidewalks and shared-use paths via general revenue funds or special assessment districts.
  3. Repairs to sidewalks are required when there is a vertical discontinuity defect of 2 inches or more, or a dangerous condition in the sidewalk itself of a particular character other than solely a vertical discontinuity.
  4. The Public Works Department, with assistance from the Planning Department, will lead the implementation of this policy and coordinate with other departments and outside organizations.
  5. The Township will maintain a comprehensive inventory of the pedestrian and bicycling infrastructure within the Township's GIS mapping system. This information can be used by various departments to identify and prioritize projects in accordance with this policy, Master Plan, and other related planning documents.
  6. Through the private development project plan review process, installation of Complete Street elements will be incorporated during the construction phase of the private development project when appropriate.
  7. Each year, the Township will evaluate applicable transportation-related projects for safety improvements in accordance with this Complete Streets Policy. As part of the Capital Improvement Plan each year, the Public Works Department will evaluate each project for incorporating Complete Streets elements, and provide a recommendation to the Capital Improvement Committee for endorsement and Township Board for approval.
  8. Each year, the Oshtemo Maintenance Department will perform an assessment of all sidewalk and bike-shared-use path assets to determine physical condition and areas requiring maintenance or reconstruction. The Maintenance Department will be responsible for addressing identified minor safety issues (i.e., large cracks and elevation changes of 2" or more). The Public Works Department will be responsible for inspecting new sidewalk construction and reconstruction of existing sidewalks.
  9. When appropriate, the Township will review existing plans and policies related to transportation planning/improvements to ensure consistency with this Complete Streets policy.
  10. The Township will develop educational materials intended to inform elected officials, staff, and interested citizens regarding the content of Complete Streets principles and best practices for implementing this policy.
  11. The Township will coordinate transportation improvement projects involving complete street elements with outside agencies to promote the most responsible and efficient use of financial resources for activities within the public right-of-way.
  12. The Township will continue to seek public and private sources of funding to implement the elements of this Complete Streets policy. The Township will also continue to coordinate transportation-related projects, including complete streets infrastructure planning and funding, through the Kalamazoo Area Transportation Study through implementation of the Metropolitan Transportation plan and Transportation Improvement Program.
  13. The Township will create a prioritization scoring system to determine which roads have the greatest need for sidewalks when funding is available. When created, the prioritization scoring system will be attached to this Complete Streets policy as Appendix A.
  14. The Township will create and maintain an ADA transition plan to identify areas in the existing sidewalk network and access routes to government buildings that are not ADA-compliant, and create a plan to correct the deficiencies.
  15. With assistance of the Capital Improvements Committee, the Township Board shall annually determine which roads will be restored/reconstructed or which will receive Hot Mix

Commented [JB5]: Defined in Michigan Statute 691.1402a

Asphalt (HMA) or gravel overlay. Selection of projects and treatments shall be subject to available Township funds, budgeting, and scheduling. The assessment policy is outlined in Appendix B.

**Appendix A: Prioritization Scoring System**

**Appendix B: Road and Sidewalk Assessment Policy**

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**Appendix A**

**Prioritization Scoring System**

Oshtemo Township requires a process for determining the priority for building sidewalks and shared-use paths within the road right-of-way. The Public Works Department reviewed similar scoring systems from cities and townships across the Midwest and selected the factors and weighting criteria that were applicable to Oshtemo Township. The scoring system is below:

Factor	Score Parameters	Weighting Criteria
Traffic Volume	(15 = high, 10 = medium, 5 = low)	x2
Route to school or park (increases probability of children using the road)	(5 = yes, 0 = no)	
Road improvement project planned (cost savings)	(5 = yes, 0 = no)	
Zoning (proxy for population density)	(5 = Commercial and Industrial, 4 = R-4, 3 = R-3, 2 = R-2, 1 = R-1, 0 = RR, Ag)	x2
Environmental Justice Area (proxy for elderly, disabled, and low-income residents who are least likely to own a car)	(5 = yes, 0 = no)	x2

**Commented [JB6]:** This table is for demonstration purposes only. The score parameters and weighting factors have not been checked to see if it provides logically consistent results.

**Notes:**

- Traffic volume in Oshtemo Township falls into three distinct categories. High volume is categorized by average daily traffic greater than 10,000 vehicles and is found on Stadium Drive, Drake Road, West Main Street, and 9<sup>th</sup> Street between West Main and I-94. Medium Traffic is categorized as 1,000-9,999 vehicles per day and is found on roads like North 10<sup>th</sup> Street, West Michigan Ave, 11<sup>th</sup> Street, and Croyden Ave. Low traffic volume streets are categorized as less than 1,000 vehicles per day which represents typical neighborhood streets.
- Traffic volume has a 2x weighting because traffic volume is highly correlated to vehicle crashes, with and without pedestrians/bicyclists.
- Zoning has a 2x weighting because more people living and working in an area increases the number of pedestrians and bicyclists. A higher number of pedestrians/bicyclists increases the probability of an accident occurring with a vehicle.
- R-5 zoning is only used for mobile home communities. For R-5 zoned areas, the sidewalks will only be built on the major road adjacent to the mobile home community. The internal roads within the mobile home community, including the entrance road, are private roads, and the Township is not responsible for building sidewalks on private roads.
- Environmental justice is a term used by the federal government to determine areas of minority and/or low-income populations to ensure these populations receive the same benefits as other areas, ensure full and fair participation by all affected communities, and to minimize and mitigate disproportionately high and adverse human health and environmental effects in these communities. The environmental justice areas in Oshtemo Township are: (1) everywhere between US-131 and Drake Road, and (2) the area bounded by US-131, KL Ave, 8<sup>th</sup> Street, and

West Main Street. Environmental justice has a 2x weighting because these areas represent a higher density of college students and retirees that may not be able to afford vehicles and rely on other means of transportation.

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**Appendix B**

**Road, Sidewalk, and Shared Use Path Assessment Policy**

**ADMINISTRATIVE PROCEDURE**

With assistance of the Capital Improvements Committee, the Township Board shall annually determine which roads will be restored/reconstructed or which will receive Hot Mix Asphalt (HMA) or gravel overlay. Selection of projects and treatments shall be subject to available Township funds, budgeting and scheduling.

A project funding formula is herein outlined. Specific projects shall be approved by the Township Board. On projects with no petitions, a Township Board resolution is needed. Resolutions or petitions for Assessment Districts shall be forwarded to the Road Commission for Public Hearing.

Township road dollars shall, in general, not be spent on roadway reconstruction if the local street is not served with sanitary sewer and sanitary sewer is available to be extended to service properties along the street. This deferment of other than routine road maintenance is to facilitate the coordination of roadway work with sanitary sewer extensions to utilize Township funds in the most efficient and effective manner. If work is deferred for a local street under this policy, said local street will be planned for sanitary sewer extension within three years.

PROJECT	FUNDING FORMULA
1. Local Roads -	
Chip Seal or Seal Coat	Township Funds as supplemented with Road Commission Par Funds
HMA Overlay/Reconstruction	Township Funds as supplemented with Road Commission Par Funds
2. Plat Streets -	
Chip Seal or Seal Coat	Township Funds as supplemented with Road Commission Par Funds
Ultra-Thin HMA Paving	100% of additional cost over Township-proposed project shall be by special assessment. Cul-de-sacs shall be assessed in a manner that approximates an equitable contribution for the "pie" shaped square yardage frontage each assessable parcel (i.e., approximately



comparable to the roadway width square yardage assessed on a two-sided street).

HMA Overlay/Reconstruction	Township Funds as supplemented with Road Commission Par Funds
3. Dust Control (gravel roads) -	100% Township Funds
4. Failed Condition Local Road -	The Failed Condition Local Road Policy of the Road Commission shall be used.
5. Sidewalks/Shared Use Paths -	<p><u>\$30.00 per Front Foot for Single Family Residential Zoning</u></p> <p><u>\$35.00 per Front Foot for Commercial/Industrial Zoning</u></p> <p><u>Note 1: If developed, actual land use shall take precedent when determining the sidewalk/path assessment. When the underlying zoning allows other than single family residential, or when substantial frontage remains undeveloped, the Township Board may elect to enter into an agreement with the property owner to defer collection of the full assessment until the parcel further develops.</u></p> <p><u>Note 2: An exception shall apply to single family residential properties which are (a) located along North 10<sup>th</sup> Street, from W. Main to the Kal Haven Trail, or (b) located along an "arterial roadway" as classified by the National Functional Classification (NFC) will not be assessed. This location-based exemption is intended to recognize the subordinate neighborhood usage of the sidewalk/path, versus the facility's function as an arterial/connector.</u></p> <p><u>The Township shall pay for all residential sidewalk construction, maintenance, and reconstruction in the road right-of-way from Township Funds. All new plats shall be built with sidewalks by the developer.</u></p> <p><u>For commercial and industrial areas, the Township will pay for the construction,</u></p>

maintenance, and reconstruction of all sidewalks and shared-use paths via general revenue funds or special assessment districts.

6. Landowner-Initiated or Shared-Use Private Roads -

At its sole discretion, the Oshtemo Township Board may elect to facilitate creation of a special assessment district for private roadway improvements when shared-use or other unique conditions warrant the proposed improvements as a public interest. Properties in Private Roadway SAD's shall bare 100% of the cost for the improvements, and shall include reimbursement of the administrative costs incurred by the Township.

A petitioned Public Roadway SAD project is exempt from Township cost participation as outlined in this policy. Specifically, the Township Board shall separately determine to what extent, if any, the Township shall contribute to the project.

**ASSESSING POLICY**

1. The Township shall annually determine which roads will be restored/reconstructed or which will receive an HMA or gravel overlay subject to budget limitations.
2. Parcels or outlots not in a plat, but with frontage on a plat street would be assessed using the funding formula, but not to exceed the largest assessment in the district.
3. Parcels located on a "curved corner" will be assessed using the funding formula but not to exceed the largest assessment in the district.
4. Assessments for condominium units will assessed using the funding formula and the amount will be divided equally among the number of units.
5. The Township has the right and responsibility to set an assessment district, when it has been determined a road must be overlaid or reconstructed and funding is needed.

6. If sidewalk/shared use path project costs are less, individual assessments will be accredited accordingly.

7. If any development is required by zoning to provide a sidewalk/shared use path, the development bears the full cost.

8. The Township reserves the right, should the need arise, to revise this policy at any time and may establish assessment districts calling for abutting land owners to share in the construction, restoration/reconstruction, HMA or gravel overlay of any road. Additionally, the Township may consider a petition for road improvements and the establishment of a special assessment as provided by state law.

DRAFT

### Annual Snow Removal Cost Estimate

The Township staff currently clear the snow from the 10-foot path on both sides of West Main Street between US-131 and the Township Hall. The effort to clear this path was used to extrapolate future snow removal efforts.

West Main Street Snow Removal per each snow event

\$38/hour fully burdened labor cost

2.38 miles total length of 10-foot path

6 hours – time to set up machines, clear path, and put machines away

$$\frac{2.38 \text{ Miles}}{6 \text{ hours}} = 0.4 \text{ miles per hour snow removal rate}$$

$$\frac{6 \text{ hours}}{2.38 \text{ miles}} * \frac{\$38}{\text{hour}} = \$95 \text{ per mile or } 1.8 \text{ cents per foot}$$

Number of snowfall events per year of at least 1"

Times per year	# of Events
2017-2018	22
2016-2017	25
2015-2016	20
2014-2015	19
2013-2014	32
2012-2013	22
2011-2012	20
2010-2011	27
2009-2010	22
2008-2009	34
2007-2008	31
2006-2007	28
Average	25.17
Standard Deviation	5.15

Proposed Snow Removal Budget Equation

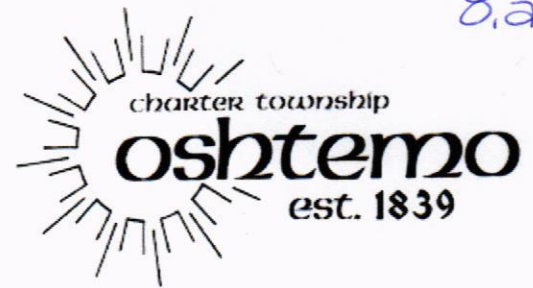
$$\frac{\text{Total Distance Sidewalks (Miles)}}{\text{miles – snow event}} * \frac{\text{Cost (\$)}}{\text{miles – snow event}} * \frac{\text{Avg Snow Events} + 2 \text{ St Dev}}{\text{Year}}$$

$$\frac{\text{Total Distance Sidewalks (miles)}}{\text{miles – snow event}} * \frac{\$95}{\text{miles – snow event}} * \frac{(25 + 10) \text{ snow events}}{\text{year}}$$

$$\text{Total Distance Sidewalks (Miles)} * \frac{\$3325}{\text{year}}$$

Year	Miles Sidewalk	Proposed Budget	Notes:
2018-2019	2.38	\$7900	No new paths added
2019-2020	2.70	\$9000	Croyden path along Farmstead added
2020-2021	3.95	\$13000	Stadium Drive added 11 <sup>th</sup> Street to DDA line
2021-2022	5.77	\$19000	9 <sup>th</sup> Street added (Quail Run to DDA line)
2022-2023	7.66	\$25500	KL Ave (Drake to Copper Beech) added
2023-2024	7.66	\$25500	No new paths added
2024-2025	10.5	\$35000	9 <sup>th</sup> Street (Quail Run to West Main)

Labor becomes the limiting factor. In 2024-2025, for each snow fall event, it will take approximately 26 man-hours (or roughly 3 man-days) to clear the 10.5 miles of paths each time it snows at least one inch.



# Memo

**To:** Oshtemo Charter Township Board  
**From:** Marc Elliott, P.E., Director of Public Works  
Jamie Baker, Public Works Technical Specialist  
**Date:** August 24, 2018  
**Subject:** Sidewalk Projects – Preliminary Overview of 2019 Work

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## OBJECTIVE

Presentation of preliminary information to support the Board's on-going discussions and decision-making regarding extent of sidewalk and non-motorized facilities to be constructed with the 2019 sewer projects.

The Board will be asked to concur with requesting its project engineer (Fleis&Vandenbrink) to provide engineering design costs for consideration at the Board's regularly scheduled September 11<sup>th</sup> meeting.

## BACKGROUND

Next week the Board will begin discussions and deliberations on the 2019 budget, and an annual update to a Capital Improvements Plan (CIP). Staff has attempted to capture the board's on-going discussions by incorporating into the 2019 CIP update, planning level projections of project costs. A preliminary draft of the CIP for non-motorized projects is provided herein as an attachment.

The attached spreadsheet provides color-bar highlights for select 2019 work. These are items of work currently being considered for inclusion with scheduled 2019 sanitary sewer constructions. When originally conceived, new sidewalks and non-motorized facilities were presented to residents in May through a series of four (4) neighborhood out-reach sessions, and to the Board on June 12<sup>th</sup>. Substantial, neighborhood-based comments have been received and considered.

When initially identified as projects, the parameter of connectivity was a major consideration. As discussions have proceeded, a draft amendment to the Township's Complete Streets Policy has evolved. The CIP worksheet color-bars reflects this evolution. The proposed projects and their extent has been have been redefined (see attached illustrations). In particular, the blue and yellow classifications will require a 2018 budget amendment to complete necessary engineering design work. Consideration of budget amendments for this service is scheduled for the Board's September 11<sup>th</sup> meeting. Traffic calming enhancements shall also be presented at that time.

## DISCUSSION

The blue projects represent projects largely within the construction limits of the sewer project and provide essential connectivity out of the neighborhoods and onto major connecting streets with planned non-motorized facilities.

The yellow projects represent planned non-motorized facilities that are mostly outside the proposed construction limits and therefore warrant a more independent consideration.

**ATTACHMENTS**

CIP Table – Non-Motorized Facility (Preliminary/draft)  
Illustrations of project scopes

Year	Project	Expenditure	Twp. Cost	Twp Share	Funding Source	Description
2019	Drake Road Sidewalk: W. Main St to Green Meadows Dr.	829,440	411,980	50%	General Fund, SoDA, MDOT	Construction of a 6-foot wide shared-use non-motorized facility along the west side of Drake Road, to include some ADA corrections within existing sidewalk facilities. By agreement, the Road Commission of Kalamazoo County will serve as the MDOT Act 51 eligible local agency to receive competitively awarded monies of the Transportation Alternatives Program (TAP) of the federal Surface Transportation Block Grant. The South Drake Authority (SoDA) will re-imburse the Township for the cost of the sidewalk/bike path.
	Drake Road Shared-Use Path: Green Meadows Drive to West Michigan Avenue	1,047,600	726,800	69%	General Fund, SoDA, MDOT	Construction of a 10-foot wide shared-use, non-motorized facility along the west side of Drake Road to connect the existing path at Arboretum Parkway through to West Michigan Avenue. By agreement, the City of Kalamazoo will serve as the MDOT Act 51 eligible local agency to receive objectively scored safety improvement monies of the federal Surface Transportation Block Grant. The South Drake Authority (SoDA) will re-imburse the Township for the cost of the sidewalk/bike path.
	Drake Road Shared-Use Path: West Michigan Avenue to Stadium Drive	276,480	140,708	51%	General Fund, SoDA, MDOT	Construction of a 10-foot wide shared-use, non-motorized facility along the west side of Drake Road. This facility will connect the Safety Grant funded pathway extent north of West Michigan Avenue, and extending south to the existing shared-use, non-motorized facility at Stadium Drive. By agreement, the Road Commission of Kalamazoo County will serve as the MDOT Act 51 eligible local agency to receive competitively awarded monies of the MDOT administered TAP program under the federal Surface Transportation Block Grant (STBG). The South Drake Authority (SoDA) will re-imburse the Township for the cost of the sidewalk/bike path.
	<i>Driftwood &amp; Skyridge Traffic Safety (Non-participating Roadway Improvements)</i>	<i>180,000</i>	<i>180,000</i>	<i>100%</i>	<i>? General Fund</i>	<i>[Under consideration] The Township Board is considering the strategic investment of traffic calming and roadway safety improvements for Driftwood and Skyridge Avenues. These facilities would be built in conjunction with the scheduled extension of sanitary sewers into established, older neighborhoods without sewers.</i>
	<i>Maple Hill Drive and Croyden Avenue Sidewalk: West Main Street to Drake Road</i>	<i>433,600</i>	<i>433,600</i>	<i>100%</i>	<i>? General Fund</i>	<i>[Under consideration] The Township Board is considering the strategic investment of "Complete Streets" pedestrian facility improvements. These facilities would be built in conjunction with the scheduled extension of sanitary sewers into established, older neighborhoods without sewers.</i>
	<i>Green Meadow Road Sidewalk: Round Hill Road to Drake Road</i>	<i>427,700</i>	<i>427,700</i>	<i>100%</i>	<i>? General Fund</i>	<i>[Under consideration] The Township Board is considering the strategic investment of "Complete Streets" pedestrian facility improvements. These facilities would be built in conjunction with the scheduled extension of sanitary sewers into established, older neighborhoods without sewers.</i>
	<i>West Main Street Cycle Track: Kalamazoo Public Library to 8th St (south-side)</i>	<i>10,000</i>	<i>10,000</i>	<i>100%</i>	<i>? General Fund</i>	<i>[Under consideration] The Township Board is considering the strategic investment of "Complete Streets" pedestrian facility improvements. This facility would be built in conjunction with the scheduled extension of sanitary sewers service the Township campus with a regional sanitary sewer pump station on 7th Street.</i>
	<i>Fairgrove Street Sidewalk: Connector from Mansfield Street to Stadium Drive</i>	<i>26,000</i>	<i>26,000</i>	<i>100%</i>	<i>? General Fund</i>	<i>[Under consideration] The Township Board is considering the strategic investment of "Complete Streets" pedestrian facility improvements. These facilities would be built in conjunction with the scheduled extension of sanitary sewers into established, older neighborhoods without sewers.</i>

- Requesting Board Approval on August 28
- Will request Board Approval on September 11 (within sewer project area)
- Will request Board Approval on September 11 (outside sewer project area)

**Non-Motorized**



Year	Project	Expenditure	Twp. Cost	Twp Share	Funding Source	Description
	Whitegate Lane Sidewalk: Connector from Powderhorn Drive to 11th Street plus plat in-fill	118,000	118,000	100%	? General Fund	[Under consideration] The Township Board is considering the strategic investment of "Complete Streets" pedestrian facility improvements. These facilities would be built in conjunction with the scheduled extension of sanitary sewers into established, older neighborhoods without sewers. In addition to the plat entrance sidewalk connector, interior sidewalks (Castleton and Lamplighter Lanes) will be extended to the nearest intersecting road.
	Stadium Drive Sidewalk: from 670-ft west of Quail Run Drive to 11th Street	30,000	30,000	100%	General Fund	Engineering design, easement acquisitions and construction documents for a proposed 6-foot wide shared-use non-motorized facility to be placed along both sides of Stadium Drive. This project received state-sponsored Transportation Improvement Program (TIP) monies allocated under authority of the Kalamazoo Area Transportation Study (KATS) as a local Transportation Alternatives Project (TAP).
	Stadium Drive Sidewalk: extending the interior of the DDA district (8th Street to 670-ft west of Quail Meadows Drive)	50,000	20,000	40%	DDA	Engineering design, easement acquisitions and construction documents for a proposed 6-foot wide shared-use non-motorized facility to be placed along both sides of Stadium Drive. This project is proposed to be constructed in conjunction with the adjoining TAP funded Stadium Drive sidewalk to the east, as well as proposed DDA streetscape improvements.
	9th St Sidewalk: Prairie Ridge Elementary School to Erie Street	20,000	20,000	100%	General Fund	Concept planning, preliminary design and administrative preparation of grant application materials for a proposed 6-foot and 5-foot wide sidewalk along both sides of 9th Street to provide a "Safe Route to Schools"
	KL Avenue Shared-Use Path: Copper Beech Boulevard to Drake Road	10,000	10,000	100%	General Fund	Design coordination with MDOT's US 131 bridge replacement and Oshtemo's planned non-motorized path under the bridge ( from Copper Beech Boulevard to Drake Road). Acquisition of required easements shall also be pursued during this phase.
	Non-Motorized Facilities Maintenance, Repair and Reconstruction	20,000	20,000	100%	General Fund	Initial overall assessment of existing public sidewalk assets, plus implementation of ongoing maintenance and repair.
	<b>Total</b>	<b>3,478,820</b>	<b>2,574,788</b>	<b>74%</b>		
<b>2020</b>	Stadium Drive Sidewalk: from 670-ft west of Quail Meadows Drive to 11th Street	500,000	182,100	36%	General Fund, KATS local TAP Grant	Construction of a 6-foot wide shared-use non-motorized facility along both sides of Stadium Drive. This project received state-sponsored Transportation Improvement Program (TIP) monies allocated under authority of the Kalamazoo Area Transportation Study (KATS) as a local Transportation Alternatives Project (TAP).
	Stadium Drive Sidewalk: extending the interior of the DDA district (8th Street to 670-ft west of Quail Meadows Drive)	1,070,000	210,000	20%	? General Fund, DDA, Other Unknown	Construction of a 6-foot wide shared-use non-motorized facility along both sides of Stadium Drive. The Township is hopeful that grant funds for transportation and community improvements can be obtained by the Township or DDA as part of an overall streetscape improvements program.

Year	Project	Expenditure	Twp. Cost	Twp Share	Funding Source	Description
	11th Street, Addition of 4-ft Shoulders from Parkview Avenue to N Avenue	109,000	109,000	100%	General Fund	Shoulders to be added in conjunction with USDA funded sewer project. Roadway reconstruction is proposed to include the addition of 4-foot shoulders as a non-motorized connector between WMU and KVCC.
	11th Street Sidewalk East-Side, Parkview to N Avenue	337,000	337,000	100%	General Fund	[Future Consideration] In accordance with (draft) non-motorized plan, sidewalk on the east-side is proposed as a school connector, to be added in conjunction with USDA funded sewer project.
	9th Street Sidewalk: Prairie Ridge Elementary School to Erie Street	54,000	54,000	100%	General Fund	Easement acquisitions, final design plans and contract documents for a proposed 6-foot and 5-foot wide sidewalk along both sides of 9th Street to provide a "Safe Route to Schools"
	KL Avenue Shared-Use Path: Copper Beech Boulevard to Drake Road	25,000	25,000	100%	General Fund	Coordination with MDOT's US 131 bridge replacement and Oshtemo's planned non-motorized path under the bridge, from Copper Beech Boulevard to Drake Road. Acquisition of required easements shall also be pursued during this phase.
	KL Avenue: Autumn's Way Boulevard to 8th Street	147,000	147,000	?	? General Fund	[Future Consideration] The Township Board is developing as a "Complete Streets" policy which will guide decision making for considering whether the addition of 4-foot paved shoulders should be a roadway improvement. This segment of KL Avenue is scheduled for reconstruction with the proposed USDA financed sanitary sewer extension. If funding is available, a further extension of the shoulder would intersect with the Wolf Tree Nature Trails.
	8th Street, West Main to KL Avenue	162,000	162,000	?	? General Fund	[Future Consideration] The Township Board is developing as a "Complete Streets" policy which will guide decision making for considering whether the addition of 4-foot paved shoulders should be a roadway improvement. This segment of 8th Street is scheduled for reconstruction with the proposed USDA financed sanitary sewer extension.
	Non-Motorized Facilities Maintenance, Repair and Reconstruction	40,000	40,000	100%	General Fund	Initial infill and construction of missing extents of public sidewalk that lack connectivity. Task includes ongoing general maintenance and repair of public sidewalks and paths.
	<b>Total</b>	<b>2,444,000</b>	<b>1,266,100</b>	<b>52%</b>		
<b>2021</b>	9th Street Sidewalk: Prairie Ridge Elementary School to Erie Street	788,000	220,640	28%	? General Fund, SRTS, Other Unknown	Construction of a proposed 6-foot and 5-foot wide sidewalk along both sides of 9th Street to provide a "Safe Route to Schools"
	KL Avenue Shared-Use Path: Drake Rd to Copper Beech	220,000	220,000	100%	? General Fund, TAP, Other Unknown	Coordination with MDOT's completion of the US 131 bridge replacements. Budget anticipates Oshtemo's participation in the costs of earth retaining walls required to extend the proposed path through the bridge abutments.

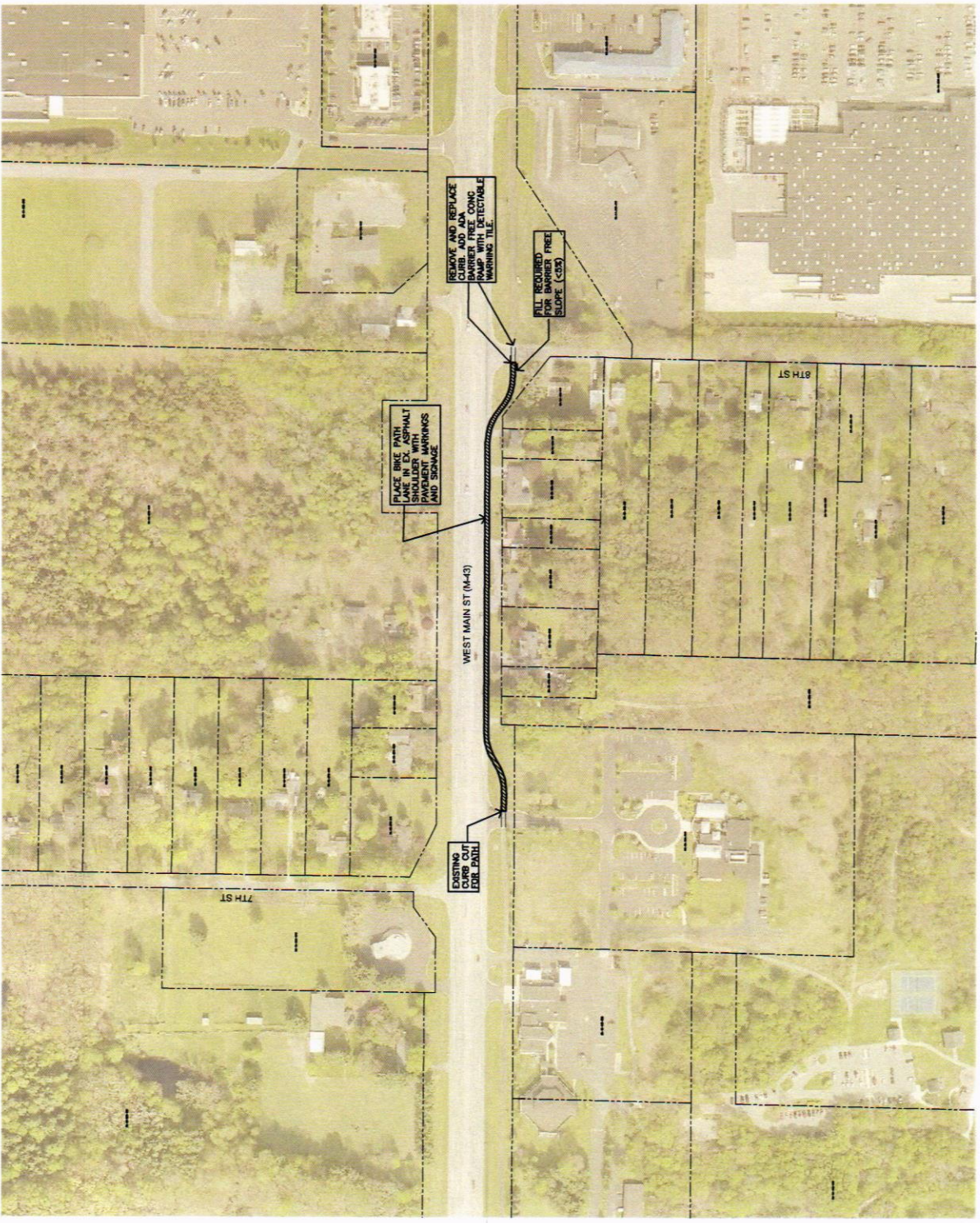
Year	Project	Expenditure	Twp. Cost	Twp Share	Funding Source	Description
	Non-Motorized Facilities Maintenance, Repair and Reconstruction	20,000	20,000	100%	General Fund	Ongoing maintenance and repair of public sidewalks and paths in the Township.
	<b>Total</b>	<b>1,028,000</b>	<b>460,640</b>	<b>45%</b>		
<b>2022</b>	KL Avenue Shared-Use Path: Drake Rd to Copper Beech	1,200,000	336,000	28%	? General Fund, TAP, Other Unknown	Construction of a 10-foot wide shared-use, non-motorized facility along the north-side of KL Avenue from Copper Beech Boulevard to Drake Road. It is anticipated that the Kalamazoo County Road Commission will serve as the MDOT Act 51 eligible local agency to grant monies of the Transportation Alter naive Program.
	9th Street Non-Motorized Facility W Main St to Prairie Ridge Elementary (TA)	42,000	42,000	100%	General Fund	Concept planning, preliminary design and administrative preparation of grant application materials for a proposed 6-foot and 5-foot wide sidewalk along both sides of 9th Street to provide a "Safe Route to Schools"
	<i>Chime and Erie Streets: Stadium Drive to 9th Street</i>	<i>95,000</i>	<i>95,000</i>	<i>?</i>	<i>? General Fund, DDA</i>	<i>[Future Consideration] The Township Board is developing as a "Complete Streets" policy which will guide decision making for considering whether the addition of 4-foot paved shoulders should be a roadway improvement. Chime Street is scheduled for reconstruction with a proposed sanitary sewer extension project. Right-of-way limitations may constrain to single side. Complete connector provides circulation between Stadium Drive and 9th Street, and connectors to Chime Elementary School and back entrance into Flesher Field Park.</i>
	Non-Motorized Facilities Maintenance, Repair and Reconstruction	20,000	20,000	100%	General Fund	Ongoing maintenance and repair of public sidewalks and paths in the Township.
	<b>Total</b>	<b>1,357,000</b>	<b>493,000</b>	<b>36%</b>		
<b>2023</b>	9th Street Non-Motorized Facility: West Main Street to Prairie Ridge Elementary School	130,000	130,000	100%	General Fund	Engineering design, easement acquisitions and construction documents for a proposed non-motorized facility to be placed along both sides of 9th Street.
	11th Street Sidewalk: KL Avenue to Stadium Drive	23,000	23,000	100%	General Fund	Concept planning, preliminary design and administrative preparation of grant application materials for a proposed sidewalk along both sides of 11th Street, to include 1 or 2 high-speed rail crossings.
	Non-Motorized Facilities Maintenance, Repair and Reconstruction	20,000	20,000	100%	General Fund	Ongoing maintenance and repair of public sidewalks and paths in the Township.
	<b>Total</b>	<b>173,000</b>	<b>173,000</b>	<b>100%</b>		

Year	Project	Expenditure	Twp. Cost	Twp Share	Funding Source	Description
<b>2024</b>	9th Street Non-Motorized Facility, W Main St to Prairie Ridge Elementary (TA)	1,918,000	537,040	28%	? General Fund, TAP, SRTS, Other Unknown	Construction of a non-motorized facility along the both sides of 9th Street. It is anticipated that funding through TAP or SRTS can be obtained.
	11th St Sidewalk, KL Avenue to Stadium Drive	190,000	190,000	100%	General Fund	Easement acquisitions, final design plans, rail crossing permits and contract documents for a proposed pedestrian facility along 11th Street.
	KL Avenue Shared-Use Path: 9th St to Copper Beech	22,000	22,000	100%	General Fund	Concept planning, preliminary design and easement needs identification to complete the 10-ft path along the north side of KL Avenue, from 9th St to Copper Beech Boulevard.
	Non-Motorized Facilities Maintenance, Repair and Reconstruction	20,000	20,000	100%	General Fund	Ongoing maintenance and repair of public sidewalks and paths in the Township.
	<b>Total</b>	<b>2,150,000</b>	<b>769,040</b>	<b>36%</b>		
	<b>Grand Total</b>	<b>10,630,820</b>	<b>5,736,568</b>	<b>54%</b>		

DRAFT



**LEGEND**  
 PROPOSED APPROXIMATE  
 TO WIDE ASPHALT





**NOT FOR CONSTRUCTION**

LEADER  
DATE: 7/10/18  
BY: P. J. J.

DATE: 7/10/18  
BY: P. J. J.

OSHTEMO TOWNSHIP  
NEIGHBORHOOD SIDEWALK DISCUSSION  
KALAMAZOO COUNTY, MICHIGAN

**SKYRIDGE - ALTERNATE PEDESTRIAN OPTION**

**FLEISCHMANN & WANDENBRINK**  
DESIGN. BUILD. OPERATE.

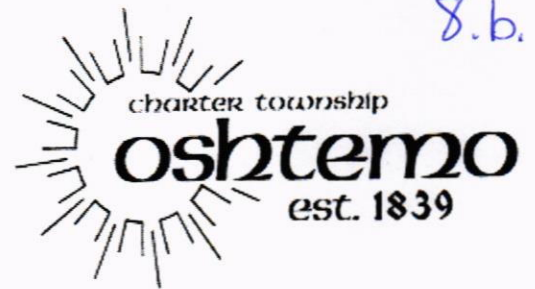
**Whitegate Farms Conceptual Sidewalk Plan will be available at the Board Meeting**

Plan includes sidewalk on Whitegate Lane from Powderhorn to Michigan Ave (east side only) and north side of Michigan Ave to 11th St.

Plan includes sidewalk on Lamplighter and Castleton (both sides) extending existing sidewalk to Powderhorn to improve ADA accessibility.

**Fairlane Conceptual Sidewalk Plan will be available at the Board Meeting**

Plan includes sidewalk on Fairgrove St between Stadium Drive and Mansfield St (east side only)



# Memo

**To:** Oshtemo Charter Township Board

**From:** Marc Elliott, P.E., Director of Public Works  
 Jamie Baker, Public Works Technical Specialist

**Date:** August 24, 2018

**Subject:** Sidewalk Projects – Authorize Additional Scope of Work

## OBJECTIVE

Board Approval of a scope of work amendment to the existing engineering services contract with Fleis&Vandenbrink (F&V) to complete the West Main Street Cycle Track design and prepare permit application materials for submittal to MDOT for this project. Due to cost savings, this additional task can be completed by F&V within the Board’s approved expenditures and the current F&S contracted cost limits.

## BACKGROUND

On March 13, 2018 that board approved contracting with F&V for preliminary design and concept planning services in order to consider the addition of non-motorized facilities as “complete streets” work. This information would provide a basis for the Board to further consider whether sidewalk construction should be included within the work-limits of the USDA financed sanitary sewer project. The sewer project (and any additional “complete streets” work) is scheduled to go to bid in January 2019.

The initial concept plans for new sidewalk and non-motorized facilities was subsequently presented to residents in a series of four (4) neighborhood out-reach sessions and before the Board on June 12th. Substantial, neighborhood-based comments have been received and considered. The proposed West Main Street Cycle Track was without controversy and universally endorsed.

## DISCUSSION

The West Main Street Cycle Track is located in the right-of-way of M-43 and will need MDOT review and permitting. Current scheduling requires expedited completion of full-design work (Cycle Track and Sewer) so that construction plans can be submitted to MDOT for review. F&V has offered to do this additional work within the previously budgeted, not-to-exceed, dollar amount of the currently contracted engineering concept planning services (see attached letter).

## ATTACHMENTS

1. West Main Cycle Track Conceptual Design
2. Letter, August 22, 2018, Fleis&Vandenbrink, Proposal to add additional scope within existing budget.





August 22, 2018

Elizabeth A. Heiny-Cogswell, Supervisor  
Oshtemo Charter Township  
7275 W. Main Street  
Kalamazoo, Michigan 49009

**RE: Neighborhood Sewer Extensions Phase 1 – West Main Street Sidewalk**

Dear Libby:

On March 14, 2018, Oshtemo Township authorized a Not to Exceed contract for \$35,000 for preliminary sidewalk work. The original scope of services has been completed with approximately \$15,000 remaining in the contract. While sidewalk discussions are ongoing for several neighborhoods, the additional trail and cycle track on West Main Street were well received.

To keep the project on schedule for a January bid opening, additional work is required to receive a Michigan Department of Transportation (MDOT) Right-of-Way use permit for the West Main Street work. We propose an additional scope to the existing \$35,000 to apply for an MDOT permit.

Fleis and VandenBrink will team with O'Boyle, Cowell, Blalock, & Associates, Inc (OCBA) of Kalamazoo to assist with the Landscape Architecture services.

#### ORIGINAL SCOPE OF SERVICES

- Perform supplemental topographic survey, where needed, in the areas where neighborhood sidewalks are proposed
- OCBA staff will provide concept drawings of sidewalk locations and estimated construction cost
- Attend one meeting with the Township to discuss progress, cost, and receive feedback on plans
- Attend one meeting with Township and USDA to coordinate sidewalk installation as part of the USDA sewer installation

#### PROPOSED SCOPE OF SERVICES

- Complete the West Main Street Trail and Cycle Track design.
- Include the trail and cycle track in the MDOT Right-of-Way permit application.

#### FEES

Fleis & VandenBrink will complete the proposed services on an hourly rate basis within the \$35,000 approved contract. F&V will bill the Township at hourly rates for time worked plus 1.1 times the cost of OCBA and reimbursable expenses. The total estimated expense is \$5,000

4798 Campus Drive  
Kalamazoo, MI 49008  
P: 269.385.0011  
F: 269.382.6972  
[www.fveng.com](http://www.fveng.com)

Please feel free to call with any questions.

Sincerely,

FLEIS & VANDENBRINK



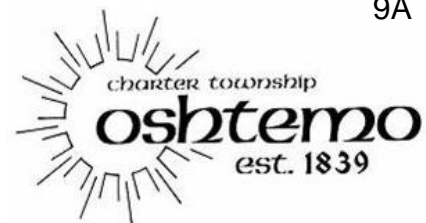
Matt G. Johnson, PE  
Project Manager

**Authorized to Proceed:**

The above scope of work is authorized as an amendment to the existing contract approved on March 14, 2018.

\_\_\_\_\_  
Oshtemo Charter Township

\_\_\_\_\_  
Date



# Memorandum

**Date:** 8/22/2018  
**To:** Township Board  
**From:** Dusty Farmer, Clerk  
**Subject:** 2018 Noticing Costs

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Township Board Members:

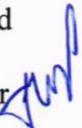
After discovering that the most recent ordinance adoptions and subsequent publication costs for noticing of those ordinance adoptions, Attorney Porter and I spoke about how to proceed with a noticing policy that is more cost effective. Attorney Porter agreed to research our options, and his memo is attached for your review.

The Clerk's Office recommendation is to notice the adoption of general ordinances through our website and our notice board at the Township Hall.

The last ordinance adoption was so expensive that a budget amendment is required. The budget amendment request in the amount of \$10,000 for publication costs is also attached.

Core Values Recognized: Integrity, Innovation, Fiscal Responsibility

# MEMORANDUM

**TO:** Township Board  
**FROM:** James W. Porter   
**DATE:** August 21, 2018  
**SUBJECT:** PUBLICATION COSTS

As all of you are aware, the Township is required to follow state law with regard to publishing certain notices as well as notices of ordinance submittal or adoption. However, it has been some time since the Board has discussed this issue as a whole. Given the recent escalating costs of publication, your Clerk thought it would be appropriate for the Board to review its current policies.

When last discussed as a Board, it was the consensus of the Board, with regard to ordinances, to publish a notice of ordinance submittal and a notice of ordinance adoption in the Kalamazoo Gazette. That was in addition to posting at the Township Hall and on the Township's website. However, that is not all required by Michigan law.

As a Charter Township we cannot pass an ordinance at the meeting at which it is first introduced, or before it is "published" in the form in which it is introduced. MCL 42.20. However, ordinances and their publication within the bounds of Michigan law are established by the Township Board. MCL 42.22.

Under Michigan law the Board is to determine the method of publication of all notices, ordinances, and proceedings. MCL 42.8. MCL 42.8 also provides that in making that determination, the Board shall choose one or both of the following methods of publication:

a. Publication in a newspaper published and circulated within the Township;

or

b. i. Posting in the office of the township and in five other public places,

OR

ii. Posting in the office of the clerk and on the township website.

As I indicated, in the past we have both published and posted in accordance with the Michigan Statute. However, that is not required. If it chose, the Board, for general ordinances, could post notice of ordinance submittal, and notice of ordinance adoption in the office of the clerk and put it on the Township website in lieu of any publication in the Gazette. This would save a substantial sum of money. The Board should discuss the method of posting. A full post of the entire ordinance rather than just a summary may be best, as well as posting on the Township's website.

The only exception to this new policy would be the adoption of zoning ordinance changes. With the adoption of the new Michigan Zoning Enabling Act (MZEA), Section 401 requires that the notice of ordinance adoption would still have to be published in a newspaper of general circulation and that Act supersedes the Charter Township Act. MCL 125.3401(10) states that the filing and publication requirements under the MZEA supersede all other statutory charter requirements relating to the filing and publication of county, township, city or village ordinances. However, we would still avoid the cost of publishing the notice of ordinance submittal in the newspaper.

This is a decision that requires Township Board approval. This is clearly a policy decision to be made by the Board. Since the issue has not been discussed for some time, the Clerk thought it appropriate to raise it in light of the ever-increasing costs of publication.

# BUDGET AMENDMENT REQUEST

(Requesting funds for a line item in addition to the approved budget)

Date: \_\_\_\_\_

Department Head Name: \_\_\_\_\_

Fund Name: \_\_\_\_\_

Amount

Additional Funds Request for: (description and GL number)	_____	_____
	_____	_____
	_____	_____

Funds requested from: (description and GL number)	_____	_____
	_____	_____
	_____	_____
	_____	_____

Explanation of request:

Supervisor Review: \_\_\_\_\_  
(pending or date reviewed)

Board Authorization: \_\_\_\_\_  
(pending or date authorized)